



Flagstone Development, Stage 4

Road Traffic Noise Intrusion Assessment

Peet Flagstone City Pty Ltd

Level 2, 167 Eagle Street
Brisbane QLD 4000

Prepared by:

SLR Consulting Australia

Level 16, 175 Eagle Street, Brisbane QLD 4000,
Australia

SLR Project No.: 620.v10512.02006

Client Reference No.: NA

25 October 2024

Revision: 4.0

**PLANS AND DOCUMENTS
referred to in the PDA
DEVELOPMENT APPROVAL**

Approval no: DEV2012/403/191

Date: 24 January 2025



Revision Record

| Revision | Date | Prepared By | Checked By | Authorised By |
|----------|-------------------|-------------------|-------------|---------------|
| 4.0 | 25 October 2024 | Rodrigo Olavarria | Steve Henry | Steve Henry |
| 3.0 | 24 October 2024 | Rodrigo Olavarria | Steve Henry | Steve Henry |
| 2.0 | 16 September 2024 | Rodrigo Olavarria | Steve Henry | Steve Henry |
| 1.0 | 22 July 2024 | Rodrigo Olavarria | Steve Henry | Steve Henry |
| | | | | |

Basis of Report

This report has been prepared by SLR Consulting Australia (SLR) with all reasonable skill, care and diligence, and taking account of the timescale and resources allocated to it by agreement with Peet Flagstone City Pty Ltd (the Client). Information reported herein is based on the interpretation of data collected, which has been accepted in good faith as being accurate and valid.

This report is for the exclusive use of the Client. No warranties or guarantees are expressed or should be inferred by any third parties. This report may not be relied upon by other parties without written consent from SLR.

SLR disclaims any responsibility to the Client and others in respect of any matters outside the agreed scope of the work.



Table of Contents

| | |
|--|-----------|
| Basis of Report | i |
| 1.0 Introduction | 1 |
| 2.0 Noise Assessment Criteria | 2 |
| 3.0 Noise Assessment Methodology | 6 |
| 3.1 Road Traffic Noise Modelling..... | 6 |
| 3.1.1 Road Traffic Noise Model Verification..... | 9 |
| 4.0 Noise Assessment Results | 10 |
| 4.1 QDC MP4.4 Noise Categories..... | 10 |
| 5.0 Recommendations | 21 |
| 5.1 Noise Barriers | 21 |
| 5.2 QDC MP4.4 Noise Category Requirements..... | 23 |
| 6.0 Conclusion | 24 |

Tables in Text

| | | |
|---------|---|----|
| Table 1 | PDA Development Approval - Condition 35 | 2 |
| Table 2 | QDC MP4.4 Noise Categories and Minimum Noise Reduction for Road Transport Noise | 4 |
| Table 3 | 2041 Road Traffic Forecasts..... | 7 |
| Table 4 | Road Traffic Noise Predictions on Lots with a Noise Barrier | 10 |

Figures in Text

| | | |
|----------|---|----|
| Figure 1 | 2041 Traffic volume estimates (Source: Project traffic consultant) | 8 |
| Figure 2 | QDC MP4.4 Noise Categories, Road Traffic Noise – Ground Floor Noise Levels Without Noise Mitigation | 18 |
| Figure 3 | QDC MP4.4 Noise Categories, Road Traffic Noise – Ground Floor Noise Levels With 2.0m High Noise Barriers..... | 19 |
| Figure 4 | QDC MP4.4 Noise Categories, Road Traffic Noise – First Floor Noise Levels Without Noise Mitigation | 20 |
| Figure 5 | QDC MP4.4 Noise Categories, Road Traffic Noise – First Floor Noise Levels With 2.0m Noise Barriers | 20 |
| Figure 6 | Recommended Noise Barriers..... | 22 |



Appendices

- Appendix A Stage 4 Overall Allotment Layout**
- Appendix B Schedule 2 of QDC MP4.4**
- Appendix C QDC MP4.4 Noise Predictions**
- Appendix D Reference Noise Barrier Designs**
- Appendix E Glossary of Terms**



1.0 Introduction

SLR Consulting Australia Pty Ltd (SLR) was commissioned by Peet Flagstone City Pty Ltd to conduct a road traffic noise intrusion assessment for the Flagstone Stage 4 development. This report addresses the road traffic noise intrusion onto residential sub-stages 4A, 4B, 4C, 4D, 4F, 4G, 4H, 4I, 4J, 4K and 4L.

The purpose of this assessment is to present a set of noise prediction results and applicable Queensland Development Code Mandatory Part 4.4 (QDC MP4.4) Noise Categories for the lots, following detailed noise modelling of road traffic noise intrusion, which was based on the latest plan of subdivision (reproduced in **Appendix A**).

Results of noise intrusion predictions are presented in this report with and without the implementation of noise barriers in front of the lots most exposed to the transportation noise sources.

Applicable Noise Categories (NC) as per the QDC MP4.4 were derived from the noise predictions for each lot with and without noise barriers.

The noise predictions are based on the latest digital elevation model and transportation volumes available on the date of issue of this report, including road traffic volume forecasts for the period coinciding 10 years after the project completion date, conservatively estimated to 2041.

The outcomes presented in this report are potentially subject to change as the project progresses; therefore, the objective of this report is to support the application for Reconfiguration of Lot (ROL) by informing the effectiveness of the implementation of noise barriers and the QDC MP4.4 Noise Categories applicable to the construction of the dwellings, after the implementation of these.

The following Codes are relevant to this assessment:

- Department of Transport and Main Roads (TMR) Transport Management Code of Practice Volume 1: Road traffic noise (CoP Vol 1).
- Queensland Development Code, Mandatory Part MP 4.4 – Buildings in a Transport Noise Corridor (QDC MP4.4).

Supporting material:

- UK Department of Transport Welsh Office Calculation of Road Traffic Noise 1988 (CoRTN)

A glossary of terms used in this report are detailed in **Appendix E**.



2.0 Noise Assessment Criteria

Flagstone Stage 4 will be part of the Greater Flagstone Urban Development Area (UDA) and therefore, is to meet the requirements of the Flagstone Development Scheme (FDS). The FDS Community Safety and Community Constraints indicates that residents and other sensitive uses are to be protected from the impacts of noise from regional transport corridors. However, the document does not provide a set of objective criteria for the assessment of these.

The current acoustic requirements applied by Economic Development Queensland (EDQ) for a number of lots within the Flagstone UDA are contained in the EDQ PDA development Condition 35 of 11 October 2024, EDQ reference DEV2012/403/171.

Condition 35 of the abovementioned Decision Notice is relevant to Acoustic Compliance and is reproduced in **Table 1**. This report assumes that the same conditions are to be applied for consistency in the assessment of other Stage 4 roads carrying relevant volumes of traffic, as Stage 4 pertains the same PDA.

Table 1 PDA Development Approval - Condition 35

| Condition | Details | Timing |
|-----------|---|--|
| 35 | <p>a) Except where identified in Condition 35A or 35B, submit to EDQ Development Assessment DSDI for compliance assessment a Noise Mitigation Report, certified by a RPEQ, for all lots within 100m from Flagstonian Drive Extension (excluding Lot 50021), the future North-South Arterial road and 200m from the railway corridor achieving a ≤ 35dB(A) for 1 hour max, over a 24 hour period for all habitable rooms.</p> <p>Where a ≤ 35dB(A) for 1 hour max, over a 24 hour period for all habitable rooms cannot be achieved, the Noise Mitigation Report is to provide the proposed noise mitigation measures generally in accordance with QDC MP4.4 – Buildings in a Noise Transport Corridor. If any noise barriers are proposed, the detailed design/construction plans certified by a RPEQ are to be provided including how passive surveillance of the streetscape can be maintained.</p> <p>Note: For lots fronting Flagstonian Drive (excluding Lot 50021), the acoustic fence must be no higher than that specified in the approved plan of development.</p> <p>Note: an acoustic report may address the acoustic needs of multiple stages/sub-stages in one report.</p> | a) Prior to the commencement of site works for the relevant sub-stage. |
| | <p>b) Construct barrier(s) works generally in accordance with the certified plans submitted under part a) of this condition.</p> | b) Prior to survey plan endorsement for each relevant sub-stage. |
| | <p>c) Submit to EDQ IS 'as constructed' plans, certified by a RPEQ, an asset register in a format acceptable to Council and 'Issued For Construction' plans for noise barriers within the relevant sub-stage.</p> | c) Prior to survey plan endorsement for each relevant sub-stage. |



| Condition | Details | Timing |
|--|---|--|
| 35A ¹ | Acoustic Treatment (Noise Barrier) – Sub-stages 3G, 3Fi, 3H, 5Ai, 5Aii, 5Bi, 5Bii, 5C, 5D, 5Ei, 5Eii, 5Eiii, 5F, 5G, 5H, 5Ki, 5Kii, 5L, 5M, 5Qii, 5R and 5S | |
| 35B ² | Acoustic Treatment (Noise Barrier) – Sub-stages 4A, 4B, 4C, 4D, 4E, 4F, 4G, 4H, 4I, 4J, 4K and 4L. | |
| | a) Submit to EDQ IS detailed acoustic wall design, certified by RPEQ generally in accordance with the approved Road Traffic Noise Intrusion Assessment for Stage 4 prepared by SLR (dated 16 September 2024). The detailed designs are to show the pad height for each lot containing a noise barrier. Note: For lots fronting Flagstonian Drive, the acoustic fence must be no higher than that specified in the approved plan of development | a) Prior to survey plan endorsement for each relevant sub-stage. |
| | b) Construct barrier(s) works generally in accordance with the certified plans submitted under part a) of this condition. | b) Prior to survey plan endorsement for each relevant sub-stage. |
| | c) Submit to EDQ IS ‘as constructed’ plans, certified by a RPEQ, an asset register in a format acceptable to Council and ‘Issued For Construction’ plans for noise barriers within the relevant sub-stage. | c) Prior to survey plan endorsement for each relevant sub-stage. |
| <p>Note 1: Condition 35A is in relation to the certification of noise walls specific to sub-stages 3G, 3Fi, 3H, 5Ai, 5Aii, 5Bi, 5Bii, 5C, 5D, 5Ei, 5Eii, 5Eiii, 5F, 5G, 5H, 5Ki, 5Kii, 5L, 5M, 5Qii, 5R and 5S. Therefore, it is not considered further.</p> <p>Note 2: Condition 35B,a) refers to the previous Stage 4 transport noise report (SLR report 620.10512.00206-R13-v2.0-20240916). This report supersedes the previous report.</p> | | |

From the above, it is understood that EDQ considers the application of QDC MP4.4 at residential lots to achieve the acoustic requirements of Condition 35. Whilst QDC MP4.4 does not provide internal noise limits or targets, the minimum building constructions in QDC MP4.4 would typically achieve an internal transport noise level close to 35 dBA $L_{Aeq,1hr}$ within habitable rooms.

QDC MP4.4 applies to residential buildings that are constructed within designated Transport Noise Corridors. It is then assumed that, for the purpose of assessing transport noise, the roads assessed in this report are to be treated as a “Transport Noise Corridor”.

Under QDC MP4.4, when building in a Transport Noise Corridor, a residential building needs to achieve certain levels of noise reduction which can be achieved through incorporating appropriate building materials to the building envelope to achieve the required noise reduction in habitable rooms.

Reproduced from QDC MP4.4, the Noise Categories and associated minimum noise reduction requirements and minimum Sound Reduction Index (Rw) for external building elements are shown in **Table 2**. The Rw is a measure of the sound insulation properties of a specific building material element.

QDC MP4.4 provides acceptable forms of construction for the external elements of the building to assist in achieving a building design and construction which meets the required



noise reduction for each Noise Category. The acceptable forms of construction in MP4.4 are reproduced in **Appendix C**, noting that other forms of construction are acceptable where they achieve the required Rw rating.

Table 2 QDC MP4.4 Noise Categories and Minimum Noise Reduction for Road Transport Noise

| Noise Category | Transport Noise Level, Facade Corrected | Minimum Transport Noise Reduction for Habitable Rooms | Building External Envelope Component | Minimum Rw Required for Each Component | |
|----------------|---|---|--------------------------------------|---|---|
| 4 | Road traffic noise ≥ 73 dBA LA10(18hour) | 40 dBA | Glazing | 43 | |
| | | | External walls | 52 | |
| | | | Roof | 45 | |
| | | | Floors | 51 | |
| | | | Entry doors | 35 | |
| 3 | Road traffic noise 68 – 72 dBA LA10(18hour) | 35 dBA | Glazing | 38 | where total area of glazing for a habitable room is greater than 1.8 m ² |
| | | | | 35 | where total area of glazing for a habitable room is less than or equal to than 1.8 m ² |
| | | | External walls | 47 | |
| | | | Roof | 41 | |
| | | | Floors | 45 | |
| | | | Entry doors | 33 | |
| | | | 2 | Road traffic noise 63 – 67 dBA LA10(18hour) | 30 dBA |
| 32 | where total area of glazing for a habitable room is less than or equal to than 1.8 m ² | | | | |
| External walls | 41 | | | | |
| Roof | 38 | | | | |
| Floors | 45 | | | | |
| Entry doors | 33 | | | | |
| 1 | Road traffic noise 58 – 62 dBA LA10(18hour) | 25 dBA | | | |
| | | | 24 | where total area of glazing for a habitable room is less than or equal to than 1.8 m ² | |
| | | | External walls | 35 | |



| Noise Category | Transport Noise Level, Facade Corrected | Minimum Transport Noise Reduction for Habitable Rooms | Building External Envelope Component | Minimum Rw Required for Each Component |
|----------------|---|--|--------------------------------------|--|
| | | | Roof | 35 |
| | | | Entry doors | 28 |
| 0 | Road traffic noise ≤57 dBA LA10(18hour) | No additional acoustic treatment required – standard building assessment provisions apply. | | |



3.0 Noise Assessment Methodology

A three-dimensional noise model for Flagstone was developed to incorporate the Stage 4 site and surrounding roads. The model was developed within SoundPLAN v8.1 acoustic software to predict transportation noise intrusion.

The computer model was created as a representation of the future noise intrusion, which incorporates the following inputs:

- Calculation algorithms. SoundPLAN implementation of the following accepted standards and methodologies:
 - UK Department of Transport Welsh Office Calculation of Road Traffic Noise 1988 (CoRTN). CoRTN is widely accepted in Australia for the calculation of road traffic noise and its use is recommended in the CoP Vol 1.
- Terrain elevation. Digital Elevation Model (DEM) built from:
 - Elevation model supplied by the Project civil consultant in files:
 - Flag Stage 4 Lots and Lot Numbers in GDA 2020 - 16 10 2024.dwg
 - Elevation models supplied for the assessment of other Flagstone and New Beith stages.
 - 3D LiDAR file representing existing contours outside the study area obtained from a QLD Government website.
- Noise barriers. Where implemented, these have been digitised at the top of a retaining wall, and or on the lot boundary. 2.0m high noise barriers have been considered to reduce the QDC MP4.4 Noise Category, where practicable.
- Road traffic volumes. Provided by the traffic consultant for the 10-year after Project construction estimated date of 2041.
- Ground surface corrections – areas of soft (absorptive) and hard (reflective) ground.
- Sensitive receptors – locations where the transportation noise levels are to be assessed.

3.1 Road Traffic Noise Modelling

Road traffic noise was modelled following general guidance from the TMR CoP Vol 1.

Road traffic volumes have been provided by the Project traffic consultant and are presented in **Table 3** and **Figure 1**. The traffic forecasts for the Year 2041 have been used, which are adopted as a 10-year after construction scenario.

The 18 hour traffic volume has further been estimated from the Vehicles Per Day (VPD) traffic volume provided by the project consultant assuming 94% of traffic occurs between 6am and 12am (midnight).

All road surfaces were assumed to be Dense Graded Asphalt (DGA), as confirmed by the Project civil consultant. On this basis, a road surface correction factor of 0 dBA was applied for all road traffic noise predictions, in accordance with CoP Vol 1.

A -0.7 dBA (free field) or -1.7 dBA (1m from façade) road traffic calibration factor was applied, where applicable, in accordance with the CoP Vol 1 with a further +2.5 dB facade correction factor in accordance with the CoRTN.

The QDC MP4.4 Noise Categories were determined from the noise contour maps calculated at 1.8 m and 4.6 m above ground level as per the DEM, for the ground floor and first floor of



two storey dwellings assumed for the development, respectively. The highest Noise Category predicted onto the lot at ground floor and first floor is conservatively reported.

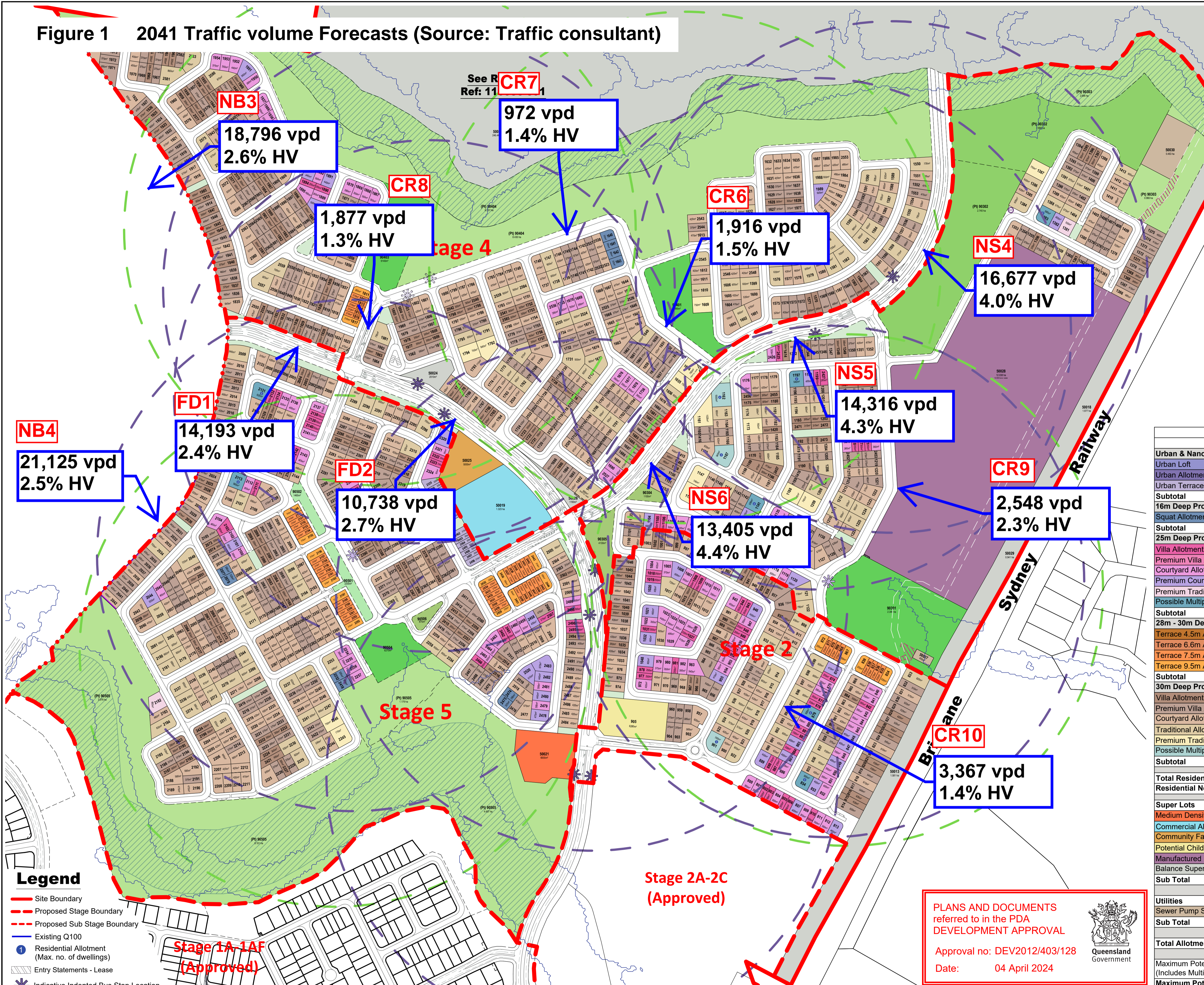
Road traffic noise contours were generated from noise predictions using a grid spacing of 0.5 m.

Table 3 2041 Road Traffic Forecasts

| Road | Segment | Vehicles per day | 18hr traffic volume estimate | % Heavy Vehicles | Posted Speed, Km/h |
|--|---------|------------------|------------------------------|------------------|--------------------|
| Flagstonian Drive | FD1 | 14,193 | 13,341 | 2.4 | 60 |
| | FD2 | 10,738 | 10,094 | 2.7 | 60 |
| New Beith Road | NB3 | 18796 | 17,665 | 2.6 | 70 |
| | NB4 | 21125 | 19858 | 2.5 | 70 |
| North South Trunk Connector | NS4 | 16,700 | 15,698 | 4.0 | 70 |
| North South Trunk Connector | NS5 | 14,316 | 13,457 | 4.3 | 70 |
| North South Trunk Connector | NS6 | 13,405 | 12,601 | 4.4 | 70 |
| Collector Road | CR6 | 1,916 | 1,801 | 1.5 | 50 |
| Collector Road | CR7 | 972 | 914 | 1.4 | 50 |
| Collector Road | CR8 | 1,877 | 1,764 | 1.3 | 50 |
| Notes: The segment naming convention is used in the SLR reports for identification purposes only. NS1-NS3 and CR1-CR5 pertain the New Beith Precincts A-B and have no influence on Flagstone Stage 4. Therefore, the traffic volumes are not presented. | | | | | |



Figure 1 2041 Traffic volume Forecasts (Source: Traffic consultant)



| Land Use | Stage 2 | Stage 3 | Stage 4 | Stage 5 | Overall | |
|--|------------------|------------------|-------------------|------------------|-------------------|--------------|
| | Area | Area | Area | Area | Area | % |
| Area of Subject Site | 16.883 ha | 45.912 ha | 301.998 ha | 52.172 ha | 416.965 ha | 100.0% |
| Saleable Area | | | | | | |
| Residential Allotments | 9.636 ha | 10.219 ha | 22.160 ha | 19.984 ha | 61.999 ha | 14.9% |
| Medium Density Allotment | — | — | 1.324 ha | — | 0.666 ha | 0.2% |
| Commercial Allotment | — | — | 0.500 ha | — | 0.500 ha | 0.1% |
| Community Facility Allotment | — | — | — | — | 0.628 ha | 0.2% |
| Potential Child Care Allotment | 0.628 ha | — | — | — | 0.628 ha | 0.2% |
| Manufactured Home Estate Allotment | — | 12.038 ha | — | — | 12.038 ha | 2.9% |
| Balance Super Allotments | 1.581 ha | 2.919 ha | 246.810 ha | — | 251.310 ha | 60.3% |
| Total Area of Allotments | 11.845 ha | 25.176 ha | 270.794 ha | 20.650 ha | 328.465 ha | 78.8% |
| Utilities | | | | | | |
| Sewer Pump Station | — | 0.508 ha | — | — | 0.508 ha | 0.1% |
| Total Area of Utilities | — | 0.508 ha | — | — | 0.508 ha | 0.1% |
| Road | | | | | | |
| Collector Road | 1.502 ha | 3.065 ha | 4.559 ha | 3.529 ha | 12.655 ha | 3.0% |
| Local Road | 3.465 ha | 4.511 ha | 9.665 ha | 8.671 ha | 26.312 ha | 6.3% |
| Linear Connections | 0.071 ha | 0.412 ha | 0.390 ha | 0.429 ha | 1.302 ha | 0.3% |
| Existing Road | — | 0.063 ha | — | — | 0.063 ha | 0.0% |
| Entry Statements | — | — | — | — | — | 0.0% |
| Total Area of New Road | 5.038 ha | 8.051 ha | 14.614 ha | 12.629 ha | 40.332 ha | 9.7% |
| Open Space | | | | | | |
| Corridor Park | — | 3.660 ha | 14.787 ha | 17.812 ha | 36.259 ha | 8.7% |
| Conservation Park (Within Corridor Park) | — | 2.650 ha | 6.357 ha | — | 9.007 ha | — |
| District Recreation Park | — | 5.648 ha | — | — | 5.648 ha | 1.4% |
| Neighbourhood Recreation Park | — | 2.346 ha | 1.803 ha | 0.627 ha | 4.776 ha | 1.1% |
| Local Recreation Park | — | 0.523 ha | — | 0.088 ha | 0.611 ha | 0.1% |
| Local Linear Recreation Park | — | — | — | 0.366 ha | 0.366 ha | 0.1% |
| Stormwater Detention | — | — | — | — | — | 0.0% |
| Total Open Space | — | 12.177 ha | 16.590 ha | 18.893 ha | 47.660 ha | 11.4% |

| Residential Allotments | Typical | | Stage 2 | Stage 3 | Stage 4 | Stage 5 | Overall | |
|--|--------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------------|-------------|
| | Typical Size | Typical Area | | | | | | |
| Urban & Nano Allotments Product | | | | | | | | |
| Urban Loft | 4.7 x 11.5m | 50m ² | — | — | — | — | 0% | |
| Urban Allotments | 7.5 x 16m | 120m ² | — | — | — | — | 0% | |
| Urban Terrace | 7.5 x 27.5m | 170m ² | — | — | — | 16 | 1% | |
| Subtotal | | | | | | 16 | 1% | |
| 16m Deep Product | | | | | | | | |
| Squat Allotment | 14 x 16m | 220m ² | — | 4 | 4 | 10 | 18 | 1% |
| Subtotal | | | | 4 | 4 | 10 | 18 | 1% |
| 25m Deep Product | | | | | | | | |
| Villa Allotment | 10 x 25m | 250m ² | 29 | 10 | 3 | 21 | 63 | 4% |
| Premium Villa Allotment | 12.5 x 25m | 313m ² | — | — | — | 3 | 3 | 0% |
| Courtyard Allotment | 14 x 25m | 350m ² | 41 | 16 | 27 | 25 | 109 | 7% |
| Premium Courtyard Allotment | 16 x 25m | 400m ² | 17 | 6 | 6 | 10 | 39 | 3% |
| Premium Traditional Allotment | 20 x 25m | 500m ² | — | 4 | — | 1 | 5 | 0% |
| Possible Multiple Residential Allotment | — | — | 2 | 3 | — | 3 | 8 | 1% |
| Subtotal | | | 89 | 39 | 36 | 63 | 227 | 15% |
| 28m - 30m Deep Product | | | | | | | | |
| Terrace 4.5m Allotment | 4.5 x 28m | 126m ² | — | — | — | — | — | 0% |
| Terrace 6.6m Allotment | 6.6 x 28m | 185m ² | — | — | — | — | — | 0% |
| Terrace 7.5m Allotment | 7.5 x 28m | 210m ² | 8 | — | 5 | 33 | 46 | 3% |
| Terrace 9.5m Allotment | 9.5 x 28m | 265m ² | 4 | — | 2 | 12 | 18 | 1% |
| Subtotal | | | 12 | — | 7 | 45 | 64 | 4% |
| 30m Deep Product | | | | | | | | |
| Villa Allotment | 10 x 30m | 300m ² | 32 | 35 | 91 | 72 | 230 | 16% |
| Premium Villa Allotment | 12.5 x 30m | 375m ² | 33 | 66 | 155 | 123 | 377 | 26% |
| Courtyard Allotment | 14 x 30m | 420m ² | 51 | 53 | 156 | 124 | 384 | 26% |
| Traditional Allotment | 20 x 30m | 600m ² | 19 | 27 | 55 | 43 | 144 | 10% |
| Premium Traditional Allotment | 25 x 30m | 720m ² | — | 7 | 5 | 2 | 14 | 1% |
| Possible Multiple Residential Allotment | — | — | 1 | 2 | — | — | 3 | 0% |
| Subtotal | | | 136 | 190 | 462 | 364 | 1152 | 78% |
| Total Residential Allotments | | | 237 | 233 | 509 | 498 | 1477 | 100% |
| Residential Net Density | | | 18.0 dw/ha | 12.9 dw/ha | 15.0 dw/ha | 16.5 dw/ha | 15.5 dw/ha | |
| Super Lots | | | | | | | | |
| Medium Density Allotment | — | — | — | — | — | 1 | 1 | |
| Commercial Allotment | — | — | — | — | 1 | — | 1 | |
| Community Facility Allotment | — | — | — | — | 1 | — | 1 | |
| Potential Child Care Allotment | — | — | — | — | — | — | — | |
| Manufactured Home Estate Allotment | — | — | — | — | — | — | — | |
| Balance Super Allotments | — | — | — | — | — | — | — | |
| Sub Total | | | 2 | 3 | 6 | 1 | 12 | |
| Utilities | | | | | | | | |
| Sewer Pump Station | — | — | 2 | — | — | — | 2 | |
| Sub Total | | | — | 2 | — | — | 2 | |
| Total Allotments | | | 239 | 238 | 515 | 499 | 1491 | |
| Maximum Potential Residential Dwellings (Includes Multiple Residential Allotments) | | | 240 | 242 | 509 | 501 | 1492 | |
| Maximum Potential Net Residential Density | | | 18.2 dw/ha | 13.4 dw/ha | 15.0 dw/ha | 16.6 dw/ha | 15.6 dw/ha | |

Legend

- Site Boundary
- Proposed Stage Boundary
- Proposed Sub Stage Boundary
- Existing Q100
- Residential Allotment (Max. no. of dwellings)
- Entry Statements - Lease
- Indicative Indented Bus Stop Location
- Indicative In-Line Bus Stop Location
- Indicative Temporary In-Line Bus Stop Location
- Bus Stop Catchment (400m)
- Neighbourhood Park Catchment (400m)
- 10m Wide Existing Sewer Easement
- SPS Access EMT
- Temporary Sewage Tankering Facility

Note:
 All dimensions and areas are approximate only, and are subject to survey and Council approval. Dimensions have been rounded to the nearest 0.1 metres. Areas have been rounded down to the nearest 5m². The boundaries shown on this plan should not be used for final detailed engineers design.

Scale: 1 : 3000 @ A1

Source Information:
 Site boundaries: Registered Survey Plans / Veris.
 Adjoining information: DCDB, Contours: Bradlees.

| | |
|------------------|--|
| REVISION | Y: 12/05/2022 Stage 5 Layout Change Z: 08/06/2022 Entry Statement Lease Areas AA: 16/06/2022 Temporary STF Notation AB: 20/09/2022 Stage 3 & 5 Layout Changes AC: 14/11/2022 Stage 5 Boundary Changes AD: 10/01/2023 Stage 3 & 4 Layout Changes AE: 24/02/2023 Stage 3 & 5 Layout Changes AF: 20/03/2023 Stage 3 & 5 Layout Changes AG: 13/04/2023 Stage 3 & 5 Layout Changes AH: 25/07/2023 Stage 4 Layout Changes AI: 08/08/2023 Stage 5 Basin Change AJ: 12/02/2024 Stage 5R Layout Change |
| PROJECT | Flagstone Precinct 1 |
| Job Ref. | 110056 |
| Date. | 12 February 2024 |
| Comp. By. | NF |
| DWG Name. | Precinct 1 Stages 2-5 |
| Chk'd By. | MD |
| Locality. | Flagstone |
| Local Authority. | Economic Development Queensland |

CLIENT

PEET

Plan of Subdivision
 Stages 2 – 5
 Overall Allotment Layout

PLANS AND DOCUMENTS referred to in the PDA DEVELOPMENT APPROVAL
 Approval no: DEV2012/403/128
 Date: 04 April 2024

Queensland Government

RPS

URBAN DESIGN
 Level 4 HQ South
 520 Wickham Street
 PO Box 1559
 Fortitude Valley QLD 4006
 T +61 7 2539 9500
 W rpsgroup.com

Scale: 1 : 3000

Sheet: A1

Plan Ref: 110056 – 374

Rev: AJ

3.1.1 Road Traffic Noise Model Verification

It is acknowledged that, according to the CoP Vol 1, a road traffic noise model is deemed to be verified if the average difference between the measured and calculated values of the noise descriptors is no more than ± 2.0 dBA.

A road traffic model verification for new roads to be built as part of the Project is not possible at this stage as the roads are non-existing. However, past experience has shown that predictions using CoRTN typically result in conservative predictions of road traffic noise levels at receptors.



4.0 Noise Assessment Results

4.1 QDC MP4.4 Noise Categories

The QDC MP4.4 Categories applicable to all lots of Stage 4 related to road traffic noise are presented in **Appendix C (Table C-1)**. The results present the lot number, stage, modelled ground elevation at the (approximate) centre of the lot and the QDC MP4.4 Noise Category for Ground Floor and First Floor noise levels (read from the noise maps as the highest Noise Category within the lot), with and without noise mitigation.

The QDC MP4.4 noise category recommended to be applied to the design of the dwelling inside the lots is derived for the residual noise level after the implementation of the recommended noise barriers shown in the noise maps.

Table 4 presents the QDC MP4.4 Categories applicable to first row lots of Stage 4 where a noise barrier is recommended in front of the lot. The results present the lot number and the QDC MP4.4 Noise Category for Ground Floor and First Floor noise levels with and without noise mitigation, i.e. noise barriers.

The modelled ground elevation at the approximate centre of the lot, as well as the approximate mid span elevation of the barrier on the lot as well as the approximate elevation of the road section immediately in front are also presented for information.

Noise contours are provided in **Figure 2** to **Figure 5** to depict the noise levels with and without the introduction of 2.0 m high noise barriers. The contours show the predicted LA10(18hour) noise level descriptor and the applicable QDC MP4.4 Categories.

A table presenting results for all noise lots is provided in **Appendix C**.

Lots predicted to achieve Noise Category “0” do not require further noise treatment to the dwelling facades.

Table 4 Road Traffic Noise Predictions on Lots with a Noise Barrier

| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 1550 | GF | 495231.53 | 6926035.67 | 49.0 | 3 | 2 | 48.8 | 46.5 | Fill |
| 1550 | FF | 495231.53 | 6926035.67 | 49.0 | 3 | 3 | | | - |
| 1551 | GF | 495232.84 | 6926018.29 | 49.0 | 3 | 2 | 48.9 | 46.7 | Fill |
| 1551 | FF | 495232.84 | 6926018.29 | 49.0 | 3 | 3 | | | - |
| 1552 | GF | 495232.17 | 6926006.47 | 49.0 | 3 | 2 | 48.5 | 46.8 | Fill |
| 1552 | FF | 495232.17 | 6926006.47 | 49.0 | 3 | 3 | | | - |
| 1553 | GF | 495229.78 | 6925994.37 | 49.1 | 3 | 2 | 48.6 | 46.9 | Fill |
| 1553 | FF | 495229.78 | 6925994.37 | 49.1 | 3 | 3 | | | - |
| 1554 | GF | 495227.87 | 6925981.01 | 49.1 | 3 | 2 | 48.7 | 47.0 | Fill |
| 1554 | FF | 495227.87 | 6925981.01 | 49.1 | 3 | 3 | | | - |
| 1555 | GF | 495224.41 | 6925964.58 | 49.2 | 3 | 2 | 48.9 | 47.2 | Fill |
| 1555 | FF | 495224.41 | 6925964.58 | 49.2 | 3 | 3 | | | - |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 1556 | GF | 495219.85 | 6925951.55 | 49.3 | 3 | 1 | 49.0 | 47.3 | Fill |
| 1556 | FF | 495219.85 | 6925951.55 | 49.3 | 3 | 3 | | | - |
| 1557 | GF | 495214.48 | 6925939 | 49.3 | 3 | 1 | 49.2 | 47.4 | Fill |
| 1557 | FF | 495214.48 | 6925939 | 49.3 | 3 | 3 | | | - |
| 1558 | GF | 495208.75 | 6925928.92 | 49.4 | 3 | 2 | 49.2 | 47.5 | Fill |
| 1558 | FF | 495208.75 | 6925928.92 | 49.4 | 3 | 3 | | | - |
| 1559 | GF | 495203.51 | 6925918.59 | 49.4 | 3 | 2 | 49.2 | 47.6 | Fill |
| 1559 | FF | 495203.51 | 6925918.59 | 49.4 | 3 | 3 | | | - |
| 1560 | GF | 495198.05 | 6925907.34 | 49.5 | 3 | 2 | 49.2 | 47.6 | Fill |
| 1560 | FF | 495198.05 | 6925907.34 | 49.5 | 3 | 3 | | | - |
| 1561 | GF | 495190.37 | 6925895.07 | 49.5 | 3 | 2 | 49.3 | 47.7 | Fill |
| 1561 | FF | 495190.37 | 6925895.07 | 49.5 | 3 | 3 | | | - |
| 1562 | GF | 494442.33 | 6925713.5 | 60.2 | 2 | 0 | 62.5 | 62.5 | Cut |
| 1562 | FF | 494442.33 | 6925713.5 | 60.2 | 3 | 2 | | | - |
| 1563 | GF | 495149.34 | 6925854.92 | 49.8 | 3 | 1 | 49.7 | 48.1 | Fill |
| 1563 | FF | 495149.34 | 6925854.92 | 49.8 | 3 | 3 | | | - |
| 1564 | GF | 495138.58 | 6925847.84 | 49.9 | 3 | 2 | 49.7 | 48.3 | Fill |
| 1564 | FF | 495138.58 | 6925847.84 | 49.9 | 3 | 3 | | | - |
| 1565 | GF | 495128.96 | 6925842.34 | 49.9 | 3 | 2 | 49.7 | 48.8 | Fill |
| 1565 | FF | 495128.96 | 6925842.34 | 49.9 | 3 | 3 | | | - |
| 1566 | GF | 495117.9 | 6925837.12 | 49.9 | 3 | 1 | 49.7 | 48.5 | Fill |
| 1566 | FF | 495117.9 | 6925837.12 | 49.9 | 3 | 3 | | | - |
| 1567 | GF | 495105.72 | 6925830.71 | 49.9 | 3 | 2 | 49.8 | 48.6 | Fill |
| 1567 | FF | 495105.72 | 6925830.71 | 49.9 | 3 | 3 | | | - |
| 1568 | GF | 495094.73 | 6925826.98 | 50.0 | 3 | 1 | 49.9 | 48.7 | Fill |
| 1568 | FF | 495094.73 | 6925826.98 | 50.0 | 3 | 3 | | | - |
| 1569 | GF | 495084.77 | 6925823.36 | 50.3 | 3 | 2 | 50.1 | 48.8 | Fill |
| 1569 | FF | 495084.77 | 6925823.36 | 50.3 | 3 | 3 | | | - |
| 1570 | GF | 495073.01 | 6925819.2 | 50.4 | 3 | 2 | 50.1 | 49.0 | Fill |
| 1570 | FF | 495073.01 | 6925819.2 | 50.4 | 3 | 3 | | | - |
| 1571 | GF | 495057.68 | 6925815.84 | 51.3 | 3 | 2 | 51.3 | 49.2 | Fill |
| 1571 | FF | 495057.68 | 6925815.84 | 51.3 | 3 | 3 | | | - |
| 1572 | GF | 495043.66 | 6925813.56 | 51.4 | 3 | 2 | 51.6 | 49.4 | Fill |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 1572 | FF | 495043.66 | 6925813.56 | 51.4 | 3 | 3 | | | - |
| 1573 | GF | 495031.53 | 6925813.03 | 51.4 | 3 | 1 | 51.5 | 49.5 | Fill |
| 1573 | FF | 495031.53 | 6925813.03 | 51.4 | 3 | 3 | | | - |
| 1574 | GF | 495020.17 | 6925812.49 | 51.2 | 2 | 1 | 51.4 | 49.6 | Fill |
| 1574 | FF | 495020.17 | 6925812.49 | 51.2 | 3 | 3 | | | - |
| 1575 | GF | 495005.67 | 6925810.96 | 51.0 | 2 | 1 | 50.8 | 49.6 | Fill |
| 1575 | FF | 495005.67 | 6925810.96 | 51.0 | 3 | 3 | | | - |
| 1654 | GF | 494874.19 | 6925708.2 | 52.0 | 3 | 2 | 52.1 | 51.3 | Fill |
| 1654 | FF | 494874.19 | 6925708.2 | 52.0 | 3 | 3 | | | - |
| 1655 | GF | 494856.23 | 6925693.3 | 52.2 | 3 | 1 | 52.4 | 51.8 | Fill |
| 1655 | FF | 494856.23 | 6925693.3 | 52.2 | 3 | 3 | | | - |
| 1656 | GF | 494844.59 | 6925677.3 | 52.5 | 3 | 1 | 52.7 | 52.4 | Fill |
| 1656 | FF | 494844.59 | 6925677.3 | 52.5 | 3 | 3 | | | - |
| 1679 | GF | 494831.82 | 6925660.73 | 52.8 | 2 | 1 | 53.0 | 52.8 | Cut |
| 1679 | FF | 494831.82 | 6925660.73 | 52.8 | 3 | 3 | | | - |
| 1680 | GF | 494823.4 | 6925650.49 | 53.0 | 2 | 1 | 53.2 | 53.2 | Cut |
| 1680 | FF | 494823.4 | 6925650.49 | 53.0 | 3 | 3 | | | - |
| 1681 | GF | 494816.58 | 6925641.55 | 53.4 | 2 | 1 | 53.5 | 53.6 | Cut |
| 1681 | FF | 494816.58 | 6925641.55 | 53.4 | 3 | 3 | | | - |
| 1682 | GF | 494810.51 | 6925633.6 | 53.7 | 2 | 1 | 53.9 | 53.9 | Cut |
| 1682 | FF | 494810.51 | 6925633.6 | 53.7 | 3 | 3 | | | - |
| 1683 | GF | 494803.28 | 6925624.24 | 54.2 | 2 | 2 | 54.3 | 54.3 | Cut |
| 1683 | FF | 494803.28 | 6925624.24 | 54.2 | 3 | 3 | | | - |
| 1684 | GF | 494796.45 | 6925613.89 | 54.8 | 2 | 2 | 55.9 | 55.9 | Cut |
| 1684 | FF | 494796.45 | 6925613.89 | 54.8 | 3 | 3 | | | - |
| 1685 | GF | 494789.3 | 6925605.78 | 55.3 | 2 | 2 | 55.5 | 55.2 | Fill |
| 1685 | FF | 494789.3 | 6925605.78 | 55.3 | 3 | 3 | | | - |
| 1686 | GF | 494783.3 | 6925597.59 | 55.8 | 2 | 2 | 56.0 | 55.7 | Fill |
| 1686 | FF | 494783.3 | 6925597.59 | 55.8 | 3 | 3 | | | - |
| 1687 | GF | 494776.31 | 6925588.51 | 56.3 | 2 | 2 | 56.5 | 56.0 | Fill |
| 1687 | FF | 494776.31 | 6925588.51 | 56.3 | 3 | 3 | | | - |
| 1688 | GF | 494769.2 | 6925578.84 | 57.0 | 3 | 2 | 57.2 | 56.4 | Fill |
| 1688 | FF | 494769.2 | 6925578.84 | 57.0 | 3 | 3 | | | - |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 1689 | GF | 494760.54 | 6925567.93 | 57.6 | 3 | 2 | 57.7 | 57.1 | Fill |
| 1689 | FF | 494760.54 | 6925567.93 | 57.6 | 3 | 3 | | | - |
| 1690 | GF | 494753.53 | 6925557.37 | 58.0 | 3 | 2 | 58.2 | 57.5 | Fill |
| 1690 | FF | 494753.53 | 6925557.37 | 58.0 | 3 | 3 | | | - |
| 1691 | GF | 494707.8 | 6925523.35 | 59.4 | 3 | 2 | 59.6 | 59.4 | Fill |
| 1691 | FF | 494707.8 | 6925523.35 | 59.4 | 3 | 3 | | | - |
| 1692 | GF | 494693.07 | 6925536.29 | 59.4 | 2 | 1 | 61.7 | 60.3 | Cut |
| 1692 | FF | 494693.07 | 6925536.29 | 59.4 | 2 | 2 | | | - |
| 1693 | GF | 494682.53 | 6925544.32 | 59.1 | 1 | 1 | 61.6 | 60.4 | Cut |
| 1693 | FF | 494682.53 | 6925544.32 | 59.1 | 2 | 2 | | | - |
| 1694 | GF | 494672.6 | 6925551.9 | 59.1 | 1 | 1 | 61.5 | 60.4 | Cut |
| 1694 | FF | 494672.6 | 6925551.9 | 59.1 | 2 | 2 | | | - |
| 1695 | GF | 494663.85 | 6925559.03 | 59.2 | 1 | 1 | 61.4 | 60.5 | Cut |
| 1695 | FF | 494663.85 | 6925559.03 | 59.2 | 2 | 2 | | | - |
| 1696 | GF | 494654.91 | 6925565.09 | 59.5 | 2 | 1 | 61.4 | 60.6 | Cut |
| 1696 | FF | 494654.91 | 6925565.09 | 59.5 | 3 | 3 | | | - |
| 1764 | GF | 494568.32 | 6925633.19 | 60.5 | 2 | 1 | 61.7 | 61.6 | Cut |
| 1764 | FF | 494568.32 | 6925633.19 | 60.5 | 3 | 3 | | | - |
| 1765 | GF | 494558.12 | 6925640.36 | 60.5 | 2 | 1 | 61.8 | 61.7 | Cut |
| 1765 | FF | 494558.12 | 6925640.36 | 60.5 | 3 | 3 | | | - |
| 1766 | GF | 494549.62 | 6925646.27 | 60.7 | 2 | 1 | 61.8 | 61.8 | Cut |
| 1766 | FF | 494549.62 | 6925646.27 | 60.7 | 3 | 3 | | | - |
| 1767 | GF | 494540.6 | 6925652.85 | 60.9 | 3 | 1 | 61.9 | 61.8 | Cut |
| 1767 | FF | 494540.6 | 6925652.85 | 60.9 | 3 | 3 | | | - |
| 1768 | GF | 494530.39 | 6925660.93 | 61.0 | 3 | 1 | 62.0 | 61.9 | Cut |
| 1768 | FF | 494530.39 | 6925660.93 | 61.0 | 3 | 3 | | | - |
| 1769 | GF | 494519.23 | 6925669.69 | 61.1 | 3 | 1 | 62.1 | 62.0 | Cut |
| 1769 | FF | 494519.23 | 6925669.69 | 61.1 | 3 | 3 | | | - |
| 1770 | GF | 494508.34 | 6925677.4 | 61.5 | 3 | 1 | 62.2 | 62.1 | Cut |
| 1770 | FF | 494508.34 | 6925677.4 | 61.5 | 3 | 3 | | | - |
| 1809 | GF | 494423.58 | 6925718.58 | 60.2 | 1 | 0 | 62.7 | 62.6 | Cut |
| 1809 | FF | 494423.58 | 6925718.58 | 60.2 | 3 | 2 | | | - |
| 1810 | GF | 494411.52 | 6925722.58 | 60.5 | 2 | 0 | 62.8 | 62.7 | Cut |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 1810 | FF | 494411.52 | 6925722.58 | 60.5 | 3 | 3 | | | - |
| 1823 | GF | 494332.78 | 6925753.21 | 62.5 | 3 | 2 | 63.4 | 63.4 | Cut |
| 1823 | FF | 494332.78 | 6925753.21 | 62.5 | 3 | 3 | | | - |
| 1824 | GF | 494318.12 | 6925757.71 | 62.5 | 3 | 1 | 63.5 | 63.6 | Cut |
| 1824 | FF | 494318.12 | 6925757.71 | 62.5 | 3 | 3 | | | - |
| 1825 | GF | 494305.83 | 6925762.5 | 62.7 | 3 | 1 | 63.6 | 63.7 | Cut |
| 1825 | FF | 494305.83 | 6925762.5 | 62.7 | 3 | 3 | | | - |
| 1826 | GF | 494294.89 | 6925766.61 | 62.8 | 3 | 1 | 63.6 | 63.7 | Cut |
| 1826 | FF | 494294.89 | 6925766.61 | 62.8 | 3 | 3 | | | - |
| 1827 | GF | 494284.64 | 6925770.88 | 63.0 | 3 | 1 | 63.7 | 63.8 | Cut |
| 1827 | FF | 494284.64 | 6925770.88 | 63.0 | 3 | 3 | | | - |
| 1828 | GF | 494273.53 | 6925775.07 | 63.1 | 3 | 1 | 63.8 | 63.9 | Cut |
| 1828 | FF | 494273.53 | 6925775.07 | 63.1 | 3 | 3 | | | - |
| 1829 | GF | 494262.26 | 6925778.88 | 63.2 | 3 | 1 | 63.9 | 64.0 | Cut |
| 1829 | FF | 494262.26 | 6925778.88 | 63.2 | 3 | 3 | | | - |
| 1830 | GF | 494252.89 | 6925783.07 | 63.4 | 3 | 1 | 64.1 | 64.1 | Cut |
| 1830 | FF | 494252.89 | 6925783.07 | 63.4 | 3 | 3 | | | - |
| 1831 | GF | 494241.6 | 6925787.23 | 63.5 | 3 | 1 | 64.1 | 64.1 | Cut |
| 1831 | FF | 494241.6 | 6925787.23 | 63.5 | 3 | 3 | | | - |
| 1832 | GF | 494231.29 | 6925791.04 | 63.6 | 3 | 1 | 64.2 | 64.1 | Cut |
| 1832 | FF | 494231.29 | 6925791.04 | 63.6 | 3 | 3 | | | - |
| 1833 | GF | 494221.59 | 6925795.31 | 63.8 | 3 | 1 | 64.3 | 64.2 | Cut |
| 1833 | FF | 494221.59 | 6925795.31 | 63.8 | 3 | 3 | | | - |
| 1834 | GF | 494210.98 | 6925798.92 | 63.9 | 3 | 1 | 64.4 | 64.4 | Cut |
| 1834 | FF | 494210.98 | 6925798.92 | 63.9 | 3 | 3 | | | - |
| 1835 | GF | 494154.1 | 6925816.59 | 64.8 | 3 | 2 | 66.7 | 66.1 | Cut |
| 1835 | FF | 494154.1 | 6925816.59 | 64.8 | 3 | 3 | | | - |
| 1836 | GF | 494151.28 | 6925830.95 | 64.5 | 2 | 1 | 66.8 | 66.1 | Cut |
| 1836 | FF | 494151.28 | 6925830.95 | 64.5 | 3 | 3 | | | - |
| 1837 | GF | 494149.78 | 6925842.6 | 64.5 | 2 | 1 | 66.8 | 66.1 | Cut |
| 1837 | FF | 494149.78 | 6925842.6 | 64.5 | 3 | 3 | | | - |
| 1838 | GF | 494149.67 | 6925854.32 | 64.6 | 2 | 1 | 66.8 | 66.2 | Cut |
| 1838 | FF | 494149.67 | 6925854.32 | 64.6 | 3 | 3 | | | - |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 1839 | GF | 494147.09 | 6925867.6 | 64.7 | 2 | 1 | 66.8 | 66.1 | Cut |
| 1839 | FF | 494147.09 | 6925867.6 | 64.7 | 3 | 3 | | | - |
| 1840 | GF | 494144.88 | 6925879.21 | 64.6 | 2 | 1 | 66.7 | 66.1 | Cut |
| 1840 | FF | 494144.88 | 6925879.21 | 64.6 | 3 | 3 | | | - |
| 1841 | GF | 494142.03 | 6925890.34 | 64.5 | 2 | 1 | 66.8 | 66.0 | Cut |
| 1841 | FF | 494142.03 | 6925890.34 | 64.5 | 3 | 3 | | | - |
| 1842 | GF | 494138.86 | 6925905.2 | 64.4 | 2 | 0 | 66.6 | 66.9 | Cut |
| 1842 | FF | 494138.86 | 6925905.2 | 64.4 | 3 | 3 | | | - |
| 1843 | GF | 494133.87 | 6925917.51 | 64.2 | 2 | 0 | 66.5 | 66.8 | Cut |
| 1843 | FF | 494133.87 | 6925917.51 | 64.2 | 3 | 3 | | | - |
| 1844 | GF | 494128.7 | 6925928.52 | 64.1 | 2 | 0 | 66.4 | 65.7 | Cut |
| 1844 | FF | 494128.7 | 6925928.52 | 64.1 | 3 | 3 | | | - |
| 1845 | GF | 494123.98 | 6925936.71 | 64.0 | 2 | 0 | 66.3 | 65.6 | Cut |
| 1845 | FF | 494123.98 | 6925936.71 | 64.0 | 3 | 3 | | | - |
| 1846 | GF | 494120.43 | 6925948.55 | 63.8 | 2 | 0 | 66.2 | 65.4 | Cut |
| 1846 | FF | 494120.43 | 6925948.55 | 63.8 | 3 | 3 | | | - |
| 1847 | GF | 494116.49 | 6925959.12 | 63.7 | 2 | 0 | 66.0 | 65.3 | Cut |
| 1847 | FF | 494116.49 | 6925959.12 | 63.7 | 3 | 3 | | | - |
| 1914 | GF | 494110.51 | 6925969.04 | 63.6 | 2 | 0 | 65.8 | 65.1 | Cut |
| 1914 | FF | 494110.51 | 6925969.04 | 63.6 | 3 | 3 | | | - |
| 1915 | GF | 494105.67 | 6925979.11 | 63.5 | 2 | 0 | 65.7 | 64.9 | Cut |
| 1915 | FF | 494105.67 | 6925979.11 | 63.5 | 3 | 3 | | | - |
| 1916 | GF | 494094.1 | 6926010.47 | 63.1 | 2 | 1 | 65.2 | 64.5 | Cut |
| 1916 | FF | 494094.1 | 6926010.47 | 63.1 | 3 | 3 | | | - |
| 1917 | GF | 494086.9 | 6926023.93 | 62.9 | 2 | 1 | 65.0 | 64.3 | Cut |
| 1917 | FF | 494086.9 | 6926023.93 | 62.9 | 3 | 3 | | | - |
| 1918 | GF | 494080.7 | 6926035.91 | 62.7 | 2 | 1 | 64.8 | 64.1 | Cut |
| 1918 | FF | 494080.7 | 6926035.91 | 62.7 | 3 | 3 | | | - |
| 1919 | GF | 494075 | 6926047.51 | 62.6 | 2 | 1 | 64.6 | 63.9 | Cut |
| 1919 | FF | 494075 | 6926047.51 | 62.6 | 3 | 3 | | | - |
| 1920 | GF | 494067.44 | 6926058.58 | 62.4 | 2 | 1 | 64.4 | 63.7 | Cut |
| 1920 | FF | 494067.44 | 6926058.58 | 62.4 | 3 | 3 | | | - |
| 1921 | GF | 494058.36 | 6926068.32 | 62.3 | 2 | 1 | 64.2 | 64.1 | Cut |

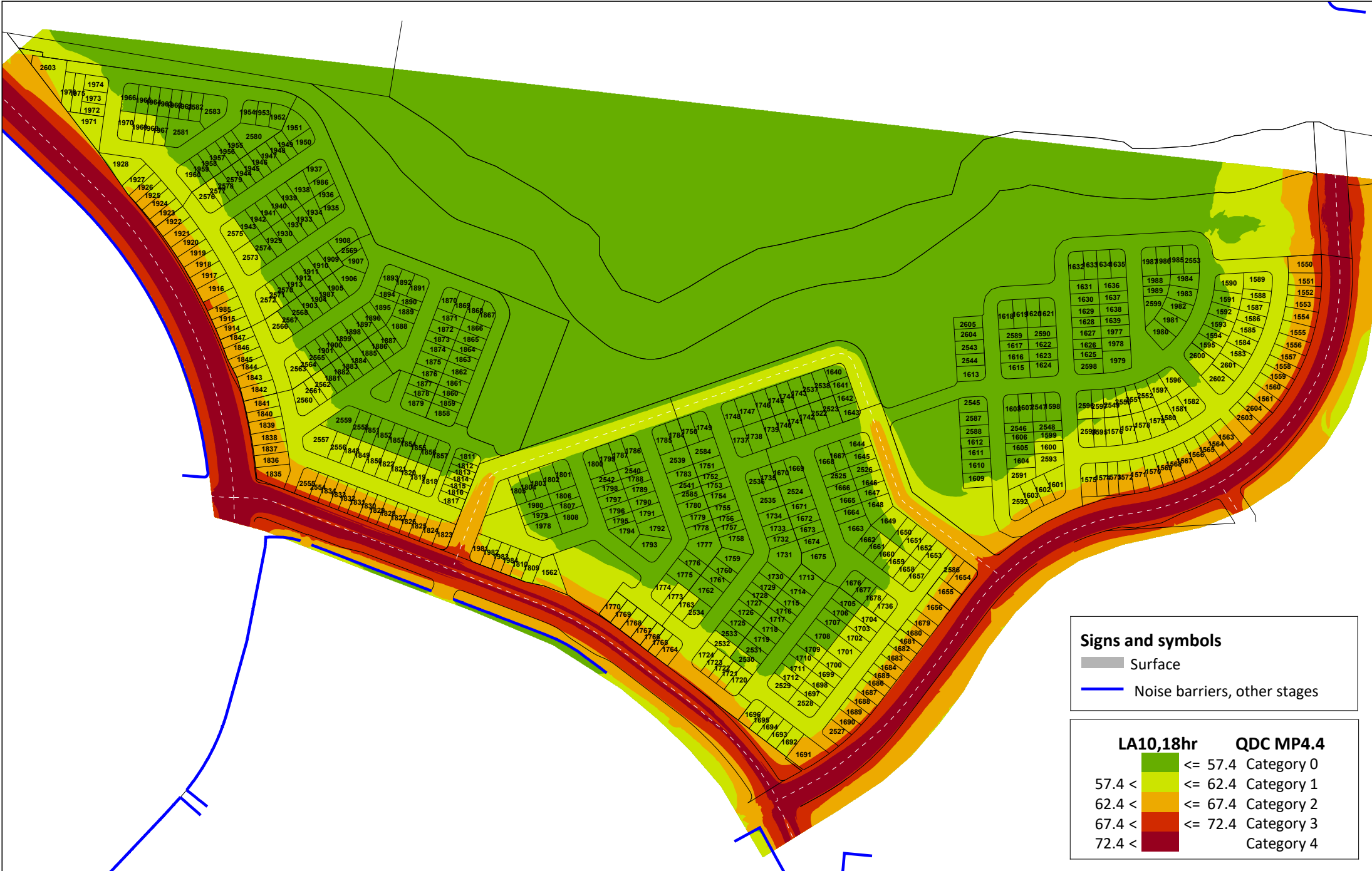


| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 1921 | FF | 494058.36 | 6926068.32 | 62.3 | 3 | 3 | | | - |
| 1922 | GF | 494049.64 | 6926080.27 | 62.1 | 2 | 1 | 64.1 | 63.3 | Cut |
| 1922 | FF | 494049.64 | 6926080.27 | 62.1 | 3 | 3 | | | - |
| 1923 | GF | 494042.87 | 6926089.75 | 61.9 | 2 | 1 | 63.9 | 63.2 | Cut |
| 1923 | FF | 494042.87 | 6926089.75 | 61.9 | 3 | 3 | | | - |
| 1924 | GF | 494035.23 | 6926099.04 | 61.6 | 2 | 1 | 63.7 | 63.0 | Cut |
| 1924 | FF | 494035.23 | 6926099.04 | 61.6 | 3 | 3 | | | - |
| 1925 | GF | 494027.46 | 6926107.93 | 61.3 | 2 | 0 | 63.5 | 62.8 | Cut |
| 1925 | FF | 494027.46 | 6926107.93 | 61.3 | 3 | 3 | | | - |
| 1926 | GF | 494019.86 | 6926116.21 | 61.3 | 2 | 0 | 63.4 | 62.7 | Cut |
| 1926 | FF | 494019.86 | 6926116.21 | 61.3 | 3 | 3 | | | - |
| 1927 | GF | 494011.41 | 6926124.4 | 60.7 | 2 | 0 | 63.2 | 62.5 | Cut |
| 1927 | FF | 494011.41 | 6926124.4 | 60.7 | 3 | 3 | | | - |
| 1928 | GF | 493994.35 | 6926140.78 | 61.9 | 1 | 0 | 64.1 | 62.2 | Cut |
| 1928 | FF | 493994.35 | 6926140.78 | 61.9 | 3 | 2 | | | - |
| 1981 | GF | 494370.31 | 6925738.6 | 61.2 | 2 | 1 | 63.1 | 63.0 | Cut |
| 1981 | FF | 494370.31 | 6925738.6 | 61.2 | 3 | 3 | | | - |
| 1982 | GF | 494381.12 | 6925734.65 | 60.9 | 2 | 1 | 63.0 | 62.9 | Cut |
| 1982 | FF | 494381.12 | 6925734.65 | 60.9 | 3 | 3 | | | - |
| 1983 | GF | 494391.36 | 6925730.04 | 60.9 | 2 | 0 | 63.0 | 62.8 | Cut |
| 1983 | FF | 494391.36 | 6925730.04 | 60.9 | 3 | 3 | | | - |
| 1984 | GF | 494401.31 | 6925726.47 | 60.8 | 2 | 0 | 62.9 | 62.7 | Cut |
| 1984 | FF | 494401.31 | 6925726.47 | 60.8 | 3 | 3 | | | - |
| 1985 | GF | 494101.27 | 6925989.11 | 63.4 | 2 | 1 | 65.5 | 64.8 | Cut |
| 1985 | FF | 494101.27 | 6925989.11 | 63.4 | 3 | 3 | | | - |
| 2527 | GF | 494743.01 | 6925547.6 | 58.4 | 3 | 2 | 58.6 | 57.9 | Fill |
| 2527 | FF | 494743.01 | 6925547.6 | 58.4 | 3 | 3 | | | - |
| 2554 | GF | 494200.28 | 6925803.05 | 64.1 | 3 | 1 | 64.6 | 64.5 | Cut |
| 2554 | FF | 494200.28 | 6925803.05 | 64.1 | 3 | 3 | | | - |
| 2555 | GF | 494189.16 | 6925806.7 | 64.2 | 3 | 1 | 64.9 | 64.9 | Cut |
| 2555 | FF | 494189.16 | 6925806.7 | 64.2 | 3 | 3 | | | - |
| 2603 | GF | 495168.77 | 6925875.48 | 49.7 | 3 | 1 | 49.5 | 48.0 | Fill |
| 2603 | FF | 495168.77 | 6925875.48 | 49.7 | 3 | 3 | | | - |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category | | Elevation at Base of Barrier (mid span), m | Approximate Elevation of Closest Road in Front of Lot, m | Lot in Cut /Fill |
|------|-------|--|------------|---------------------|---------------------------------------|---------------|--|--|------------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier | | | |
| 2604 | GF | 495178.54 | 6925885.59 | 49.6 | 3 | 1 | 49.4 | 47.9 | Fill |
| 2604 | FF | 495178.54 | 6925885.59 | 49.6 | 3 | 3 | | | - |





Signs and symbols

- Surface
- Noise barriers, other stages

| LA10,18hr | QDC MP4.4 |
|---|---------------------------|
| | <= 57.4 Category 0 |
| | 57.4 < <= 62.4 Category 1 |
| | 62.4 < <= 67.4 Category 2 |
| | 67.4 < <= 72.4 Category 3 |
| | 72.4 < Category 4 |

SLR
 L16 / 175 EAGLE STREET
 BRISBANE
 QLD 4000
 AUSTRALIA
 T: 61 7 3856 4800
 www.slrconsulting.com

SCALE :3500
 0 15 30 60 90 120 150 180 210 240 270 300
 m

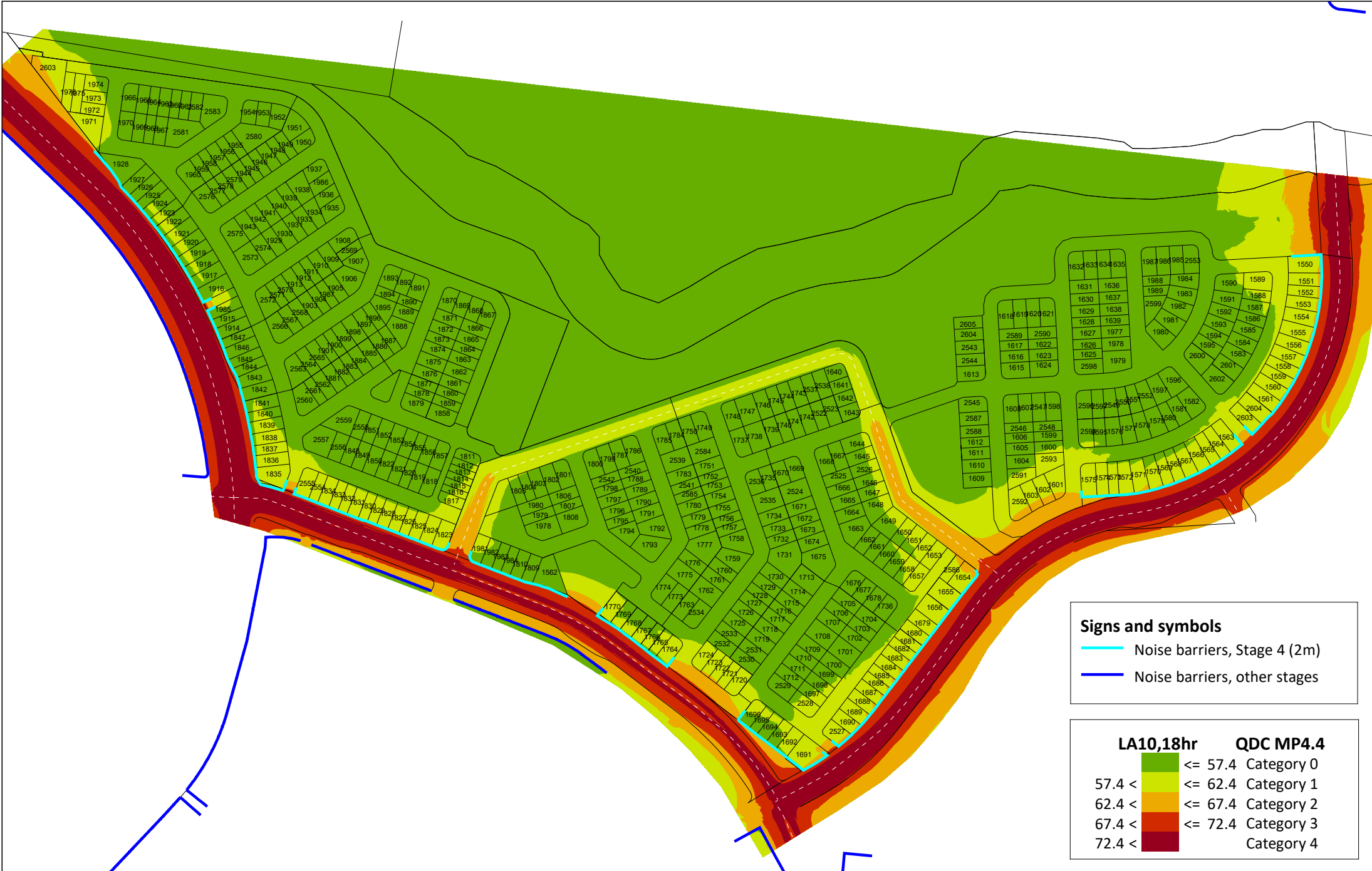


| | |
|--------------------|---|
| PROJECT | Flagstone Stage 4 |
| CLIENT | Peet Flagstone City Pty Ltd |
| DESCRIPTION | Ground Floor Facade Corrected LA10,18hr at 1.8m Above the Ground Without Noise Mitigation |

| | |
|--------------------|--------------------------|
| Date: | 23/10/2024 |
| Project No.: | 620.10512.00206 |
| Report No.: | 620.10512.00206-R13-v3.0 |
| Prediction Method: | CoRTN |
| Prepared By: | RO |
| Prediction Height: | 1.8 m |

Figure 2

The content contained within this document may be based on third party data. SLR Consulting Australia Pty Ltd does not guarantee the accuracy of any such information.



Signs and symbols

- Noise barriers, Stage 4 (2m)
- Noise barriers, other stages

| LA10,18hr | QDC MP4.4 |
|-----------|---------------------------|
| | <= 57.4 Category 0 |
| | 57.4 < <= 62.4 Category 1 |
| | 62.4 < <= 67.4 Category 2 |
| | 67.4 < <= 72.4 Category 3 |
| | > 72.4 Category 4 |

SLR
 L16 / 175 EAGLE STREET
 BRISBANE
 QLD 4000
 AUSTRALIA
 T: 61 7 3856 4800
 www.slrconsulting.com

SCALE :3500
 0 15 30 60 90 120 150 180 210 240 270 300
 m

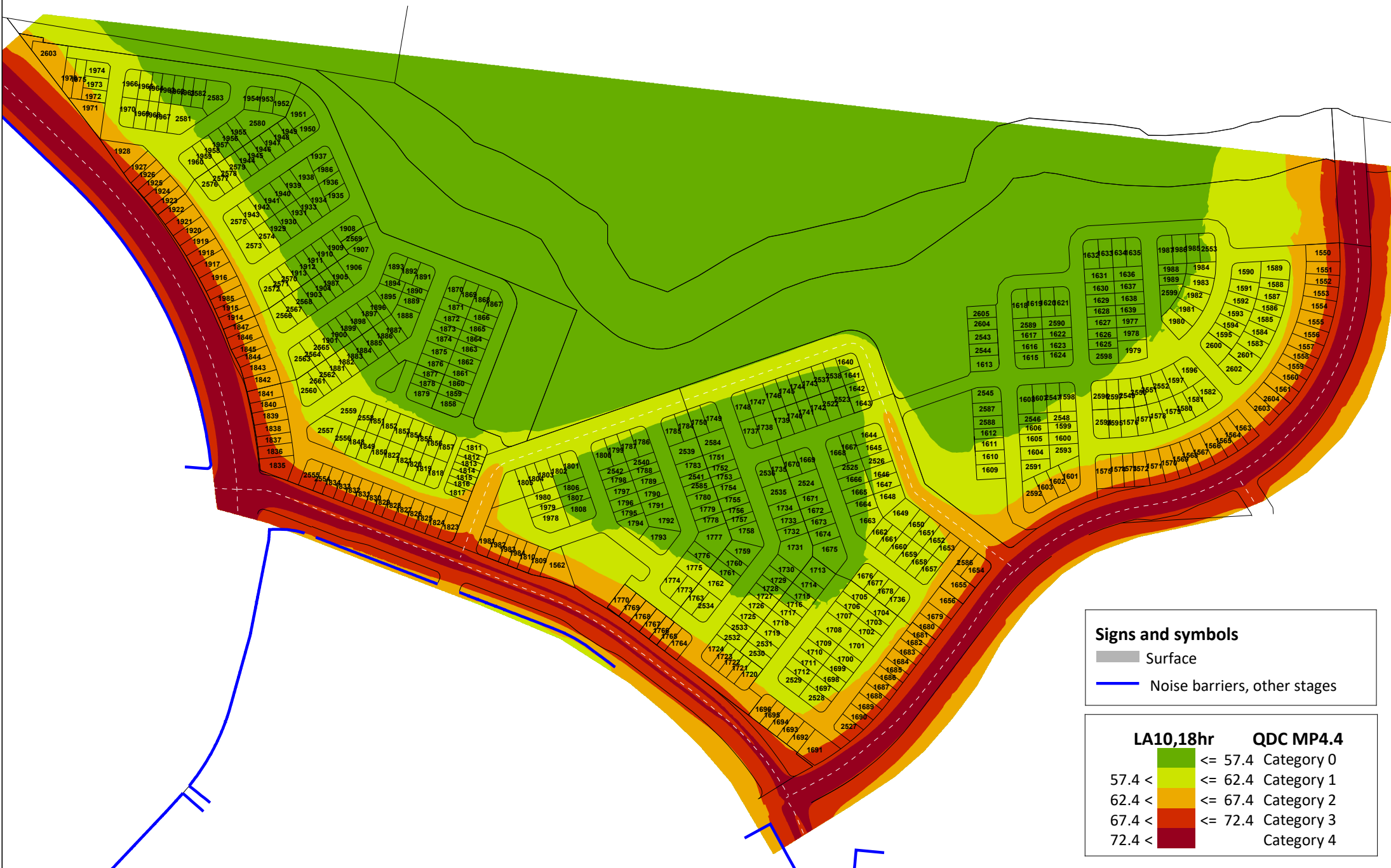


| | |
|--------------------|--|
| PROJECT | Flagstone Stage 4 |
| CLIENT | Peet Flagstone City Pty Ltd |
| DESCRIPTION | Ground Floor Facade Corrected LA10,18hr at 1.8m Above the Ground With Noise Barriers |

| | |
|--------------------|--------------------------|
| Date: | 23/10/2024 |
| Project No.: | 620.10512.00206 |
| Report No.: | 620.10512.00206-R13-v1.0 |
| Prediction Method: | CoRTN |
| Prepared By: | RO |
| Prediction Height: | 1.8 m |

Figure 3

The content contained within this document may be based on third party data. SLR Consulting Australia Pty Ltd does not guarantee the accuracy of any such information.



Signs and symbols

- Surface
- Noise barriers, other stages

| LA10,18hr | QDC MP4.4 |
|-----------|--------------------|
| <= 57.4 | Category 0 |
| 57.4 < | <= 62.4 Category 1 |
| 62.4 < | <= 67.4 Category 2 |
| 67.4 < | <= 72.4 Category 3 |
| 72.4 < | Category 4 |

SLR
 L16 / 175 EAGLE STREET
 BRISBANE
 QLD 4000
 AUSTRALIA
 T: 61 7 3856 4800
 www.slrconsulting.com

SCALE :3500
 0 15 30 60 90 120 150 180 210 240 270 300
 m

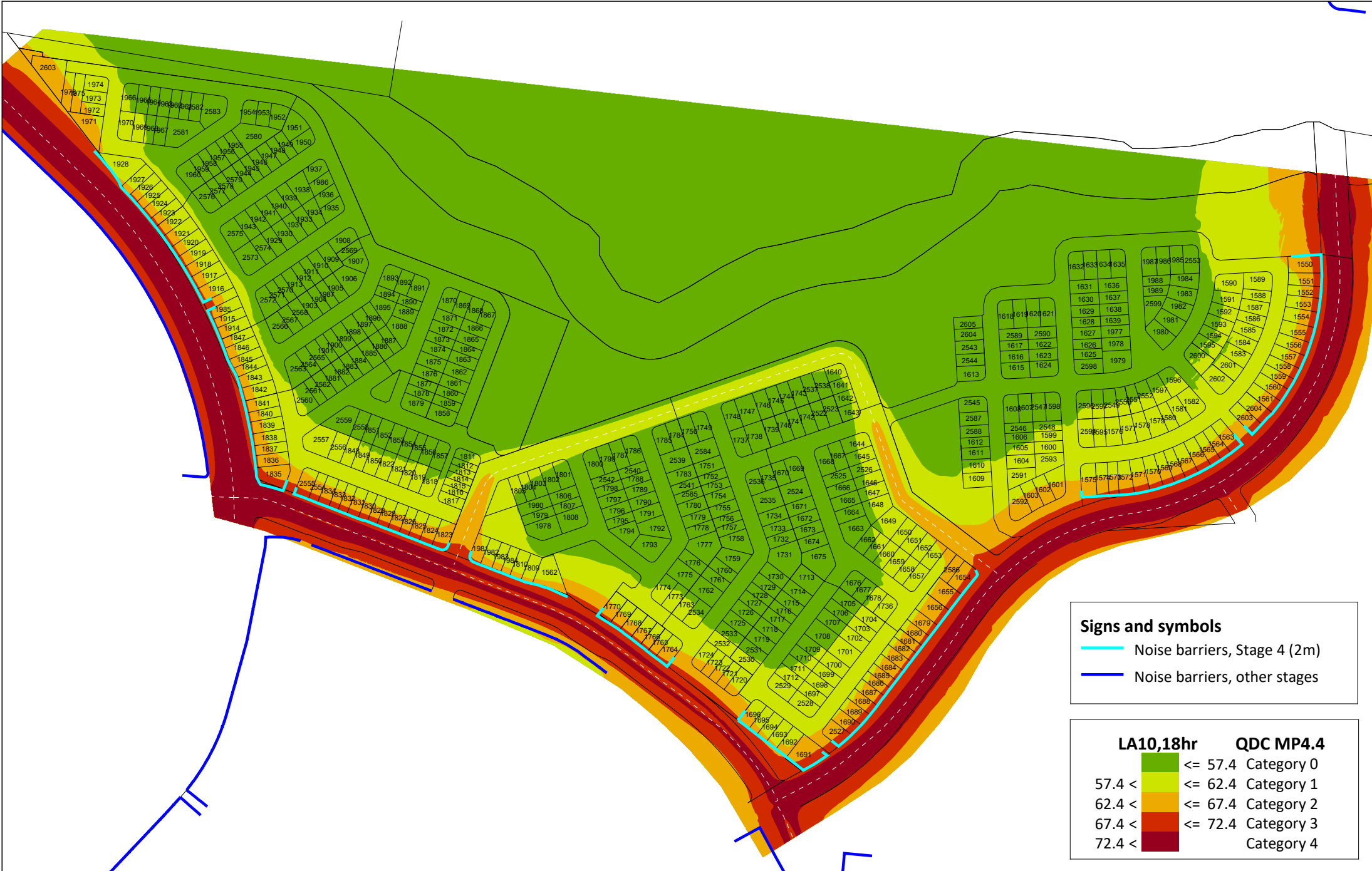


| | |
|--------------------|--|
| PROJECT | Flagstone Stage 4 |
| CLIENT | Peet Flagstone City Pty Ltd |
| DESCRIPTION | First Floor Facade Corrected LA10,18hr at 4.6m Above the Ground Without Noise Mitigation |

| | |
|--------------------|--------------------------|
| Date: | 23/10/2024 |
| Project No.: | 620.10512.00206 |
| Report No.: | 620.10512.00206-R13-v3.0 |
| Prediction Method: | CoRTN |
| Prepared By: | RO |
| Prediction Height: | 4.6 m |

Figure 4

The content contained within this document may be based on third party data. SLR Consulting Australia Pty Ltd does not guarantee the accuracy of any such information.



Signs and symbols

- ▬ Noise barriers, Stage 4 (2m)
- ▬ Noise barriers, other stages

| LA10,18hr | | QDC MP4.4 | |
|-----------|---------|------------|------------|
| | <= 57.4 | Category 0 | |
| | 57.4 < | <= 62.4 | Category 1 |
| | 62.4 < | <= 67.4 | Category 2 |
| | 67.4 < | <= 72.4 | Category 3 |
| | 72.4 < | | Category 4 |

SLR
 L16 / 175 EAGLE STREET
 BRISBANE
 QLD 4000
 AUSTRALIA
 T: 61 7 3856 4800
 www.slrconsulting.com

SCALE :3500
 0 15 30 60 90 120 150 180 210 240 270 300
 m



| | |
|--------------------|---|
| PROJECT | Flagstone Stage 4 |
| CLIENT | Peet Flagstone City Pty Ltd |
| DESCRIPTION | First Floor Facade Corrected LA10,18hr at 4.6m Above the Ground With Noise Barriers |

| | |
|--------------------|--------------------------|
| Date: | 23/10/2024 |
| Project No.: | 620.10512.00206 |
| Report No.: | 620.10512.00206-R13-v3.0 |
| Prediction Method: | CoRTN |
| Prepared By: | RO |
| Prediction Height: | 4.6 m |

Figure 5

The content contained within this document may be based on third party data. SLR Consulting Australia Pty Ltd does not guarantee the accuracy of any such information.

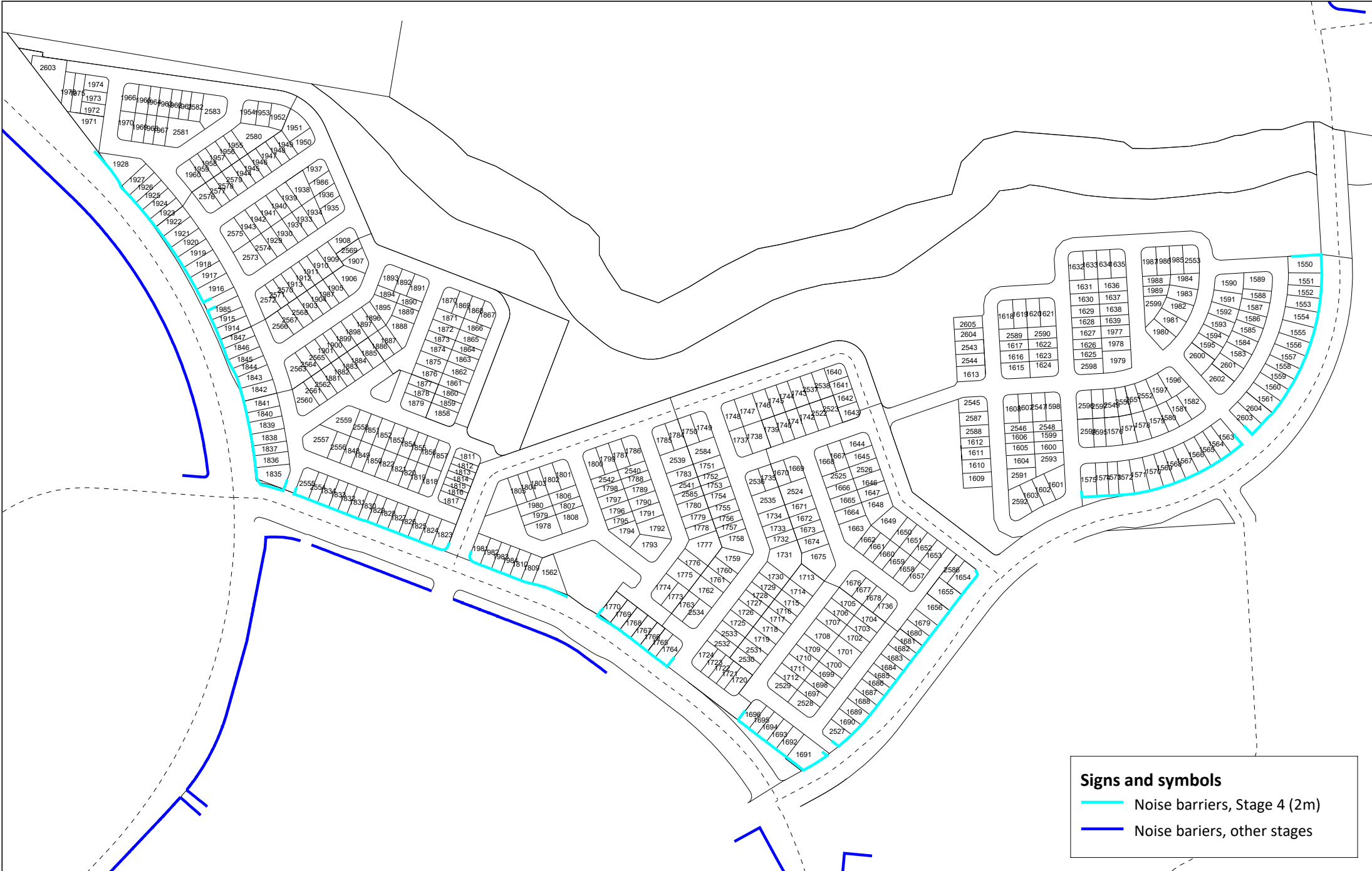
5.0 Recommendations

5.1 Noise Barriers

For the external noise levels to meet the predicted QDC MP4.4 noise categories detailed in **Section 4.0**, noise barriers are recommended to be built as follows:

- The location of the modelled 2.0 m high noise barriers is shown in **Figure 6**.
The noise barriers can be made of an earth mound, acoustic fence or a combination thereof. **Table 4** shows the approximate ground elevation at the base of the barrier at mid span of the respective lot, the ground elevation at the approximate centre of the lot and the approximate elevation of the closest road lane.
- The noise barriers modelled elevation is detailed in **Table 4**, which are to be built on top of retaining walls. This applies regardless of whether the lot is in a cut or in fill (i.e. lot at a lower elevation than the assessed road immediately adjoining, and vice versa).
- The barriers must generally be installed without gaps between panels and posts. Small gaps between the bottom of the panels are permissible if required for drainage. However, these must be minimised.
- The noise barriers must have a minimum surface density of 12.5 kg/m² (excluding structural components):
 - Overlapped timber barriers are suitable. Brisbane City Council drawings [BSD-7021](#) and Moreton Bay Regional Council drawings [SF-1520](#) are provided for reference (also reproduced in **Appendix D**). Note the noise barriers must be built to the minimum height indicated in **Figure 6**.
 - Other construction materials are also suitable where the panels (structural components excluded) meet the minimum surface density and the barrier is built following the above guidance.
- The design life of the noise barriers is to be 40 years unless otherwise agreed upon in writing with MEDQ.

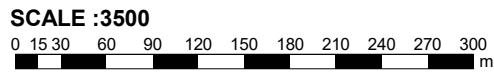




Signs and symbols

- Noise barriers, Stage 4 (2m)
- Noise barriers, other stages

SLR
 L16 / 175 EAGLE STREET
 BRISBANE
 QLD 4000
 AUSTRALIA
 T: 61 7 3856 4800
 www.slrconsulting.com



| | |
|--------------------|-----------------------------|
| PROJECT | Flagstone Stage 4 |
| CLIENT | Peet Flagstone City Pty Ltd |
| DESCRIPTION | Stage 4 Noise Barriers |

| | |
|--------------------|--------------------------|
| Date: | 24/10/2024 |
| Project No.: | 620.10512.00206 |
| Report No.: | 620.10512.00206-R13-v3.0 |
| Prediction Method: | N.A. |
| Prepared By: | RO |
| Prediction Height: | N.A. |

Figure 6

The content contained within this document may be based on third party data. SLR Consulting Australia Pty Ltd does not guarantee the accuracy of any such information.

5.2 QDC MP4.4 Noise Category Requirements

QDC MP4.4 Categories applicable to the relevant lots pertaining the assessed Stage 4 after the implementation of 2.0 m high noise barriers as specified in **Section 5.1** were presented in **Section 4.1**.

A full list of QDC MP4.4 Noise Categories applicable to all lots within Stage 4 is provided in **Table C-1 (Appendix C)**.

The Rw rating applicable to the dwelling facade elements for each of the QDC MP4.4 Categories are presented in **Table 2**. Acceptable forms of construction are reproduced from Schedule 2 of QDC MP4.4 in **Appendix B**, noting that other forms of construction are acceptable where they meet the required Rw rating.

The noise attenuation provided by the dwelling facade will be largely controlled by the window elements; therefore, it is recommended that facade glazing systems (window + frame + seals) required to achieve a minimum Rw performance are supplied with an acoustic test report conducted in Australia by a qualified consultant who is a member of the Australian Acoustical Society (AAS), or an acoustic consultant who works for a member firm of the Association of Australasian Acoustical Consultants (AAAC). The acoustic test report should address the requirements in the following standards:

- AS 1191-2002 *Acoustics – Method for laboratory measurement of airborne sound transmission insulation of building elements*
- ISO 717-1:2013 *Acoustics – Rating of sound insulation in buildings and of building elements – Part 1: Airborne sound insulation*

It should be noted that as stated in QDC MP4.4, *“the part applies to building work for the construction or renovation of a residential building in a designated transport noise corridor”*; therefore, it is only relevant at the Building Application stage of the individual lots being the building owner responsible for obtaining certification.

A lower Noise Category should be acceptable at specific facades of the future dwellings depending on the layout of these within the lots, pending demonstration of the road traffic noise levels onto specific habitable spaces within a dwelling via a lot specific noise assessment based on architectural drawings, presented by the lot owner.



6.0 Conclusion

SLR Consulting Pty Ltd (SLR) have completed a road traffic noise assessment of the proposed Flagstone Stage 4 development. This report addresses the road traffic noise intrusion onto residential sub-stage 4A, 4B, 4C, 4D, 4E, 4F, 4G, 4H, 4I, 4J, 4K and 4L.

The assessment was conducted following guidance from the Queensland Department of Transport and Main Roads (DTMR) – Transport Noise Management: Code of Practice Volume 1 - Road Traffic Noise, dated November 2013 (CoP Vol 1).

A computational noise model was used to predict future road traffic noise levels from roads that will be built as part of the Flagstone development.

For the external noise levels to meet the predicted noise categories detailed in this report, noise barriers are recommended to be built as follows:

- The location of the modelled 2.0 m high noise barriers is shown in **Figure 6**.
The noise barriers can be made of an earth mound, acoustic fence or a combination thereof. **Table 4** shows the approximate ground elevation at the base of the barrier at mid span of the respective lot, the ground elevation at the approximate centre of the lot and the approximate elevation of the closest road lane.
- The noise barriers modelled elevation is detailed in **Table 4**, which are to be built on top of retaining walls. This applies regardless of whether the lot is in a cut or in fill (i.e. lot at a lower elevation than the assessed road immediately adjoining, and vice versa).
- The barriers must generally be installed without gaps between panels and posts. Small gaps between the bottom of the panels are permissible if required for drainage. However, these must be minimised.
- The noise barriers must have a minimum surface density of 12.5 kg/m² (excluding structural components):
 - Overlapped timber barriers are suitable. Brisbane City Council drawings [BSD-7021](#) and Moreton Bay Regional Council drawings [SF-1520](#) are provided for reference (also reproduced in **Appendix D**). Note the noise barriers must be built to the minimum height indicated in **Figure 6**.
 - Other construction materials are also suitable where the panels (structural components excluded) meet the minimum surface density and the barrier is built following the above guidance.
- The design life of the noise barriers is to be 40 years unless otherwise agreed upon in writing with MEDQ.

The residual noise levels after the implementation of 2.0 m high noise barriers were assessed against noise criteria derived from the Queensland Development Code Mandatory Part 4.4 (QDC MP4.4).

QDC MP4.4 Categories applicable to all the lots pertaining to the assessed Stage 4 are presented in **Appendix C**.

The Rw rating applicable to the dwelling facade elements are presented in **Table 2**. Acceptable forms of construction are reproduced from Schedule 2 of QDC MP4.4 in **Appendix B**, noting that other forms of construction are acceptable where they meet the required Rw rating.

The predicted QDC MP4.4 Noise Categories presented in this report represent the highest Noise Category for any part of the Lot (rather than on the building envelope). A lower Noise



Category may be applicable depending on the position and layout of the building on the Lot. The constructed dwellings and other lot fences will also screen road noise. For these reasons, the Noise Categories are considered a conservative assessment of transport noise.

A lower Noise Category should be acceptable at specific facades of the future dwellings depending on the layout of these within the lots, pending demonstration of the road traffic noise levels onto specific habitable spaces within a dwelling via a lot-specific noise assessment based on architectural drawings, presented by the lot owner.





Appendix A Stage 4 Overall Allotment Layout

Flagstone Development, Stage 4

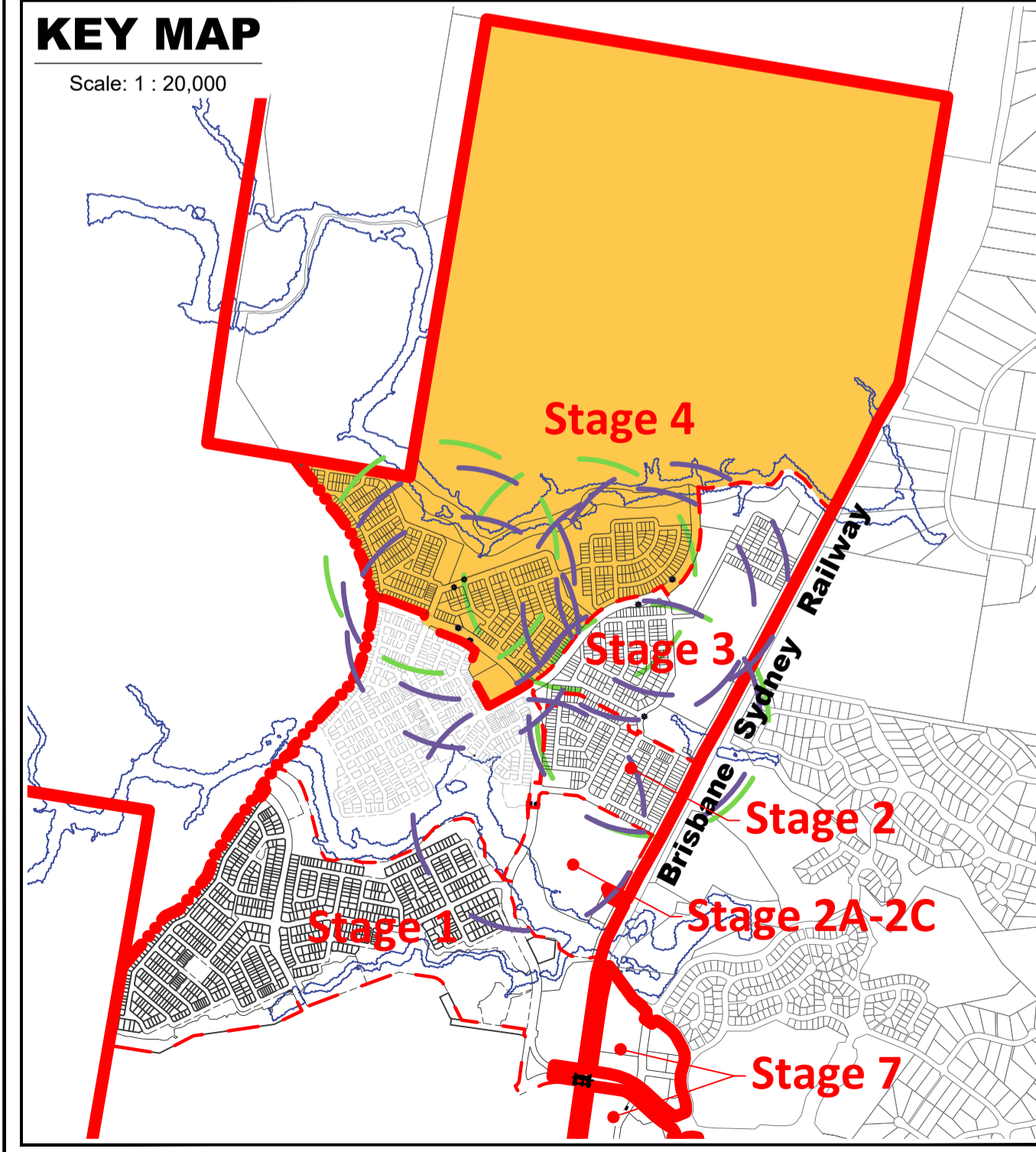
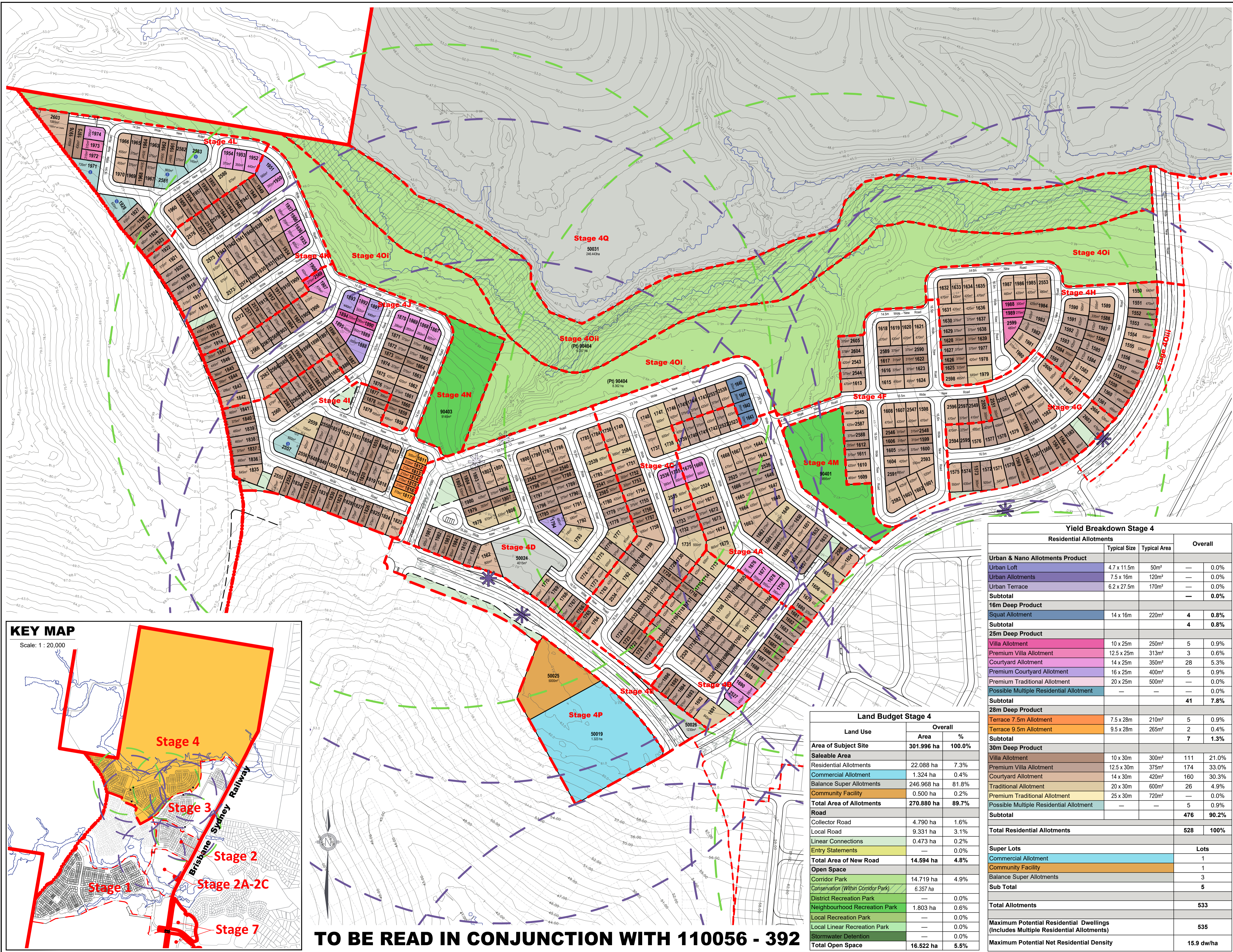
Road Traffic Noise Intrusion Assessment

Peet Flagstone City Pty Ltd

SLR Project No.: 620.v10512.02006

25 October 2024





TO BE READ IN CONJUNCTION WITH 110056 - 392

REVISION

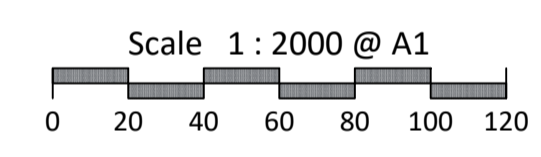
| | | |
|-----|------------|----------------------------|
| AB: | 20/09/2022 | Stage 3 & 5 Layout Changes |
| AC: | 14/11/2022 | Stage 5 Boundary Changes |
| AD: | 10/01/2023 | Stage 3 & 4 Layout Changes |
| AE: | 24/02/2023 | Stage 3 & 5 Layout Changes |
| AF: | 20/03/2023 | Stage 3 & 5 Layout Changes |
| AG: | 13/04/2023 | Stage 3 & 5 Layout Changes |
| AH: | 25/04/2023 | Stage 4 Layout Changes |
| AI: | 08/08/2023 | Stage 5 Basin Change |
| AJ: | 12/02/2024 | Stage 5R Layout Change |
| AK: | 15/08/2024 | Stage 4 Layout Change |
| AL: | 23/08/2024 | Stage 4 Layout Change |
| AM: | 27/09/2024 | Stage 4 Layout Change |

Note:
All dimensions and areas are approximate only, and are subject to survey and Council approval.
Dimensions have been rounded to the nearest 0.1 metres.
Areas have been rounded down to the nearest 5m².
The boundaries shown on this plan should not be used for final detailed engineers design.
Road linemarkings and turn slots are indicative only and subject to detailed design.

Source Information:
Site boundaries: Registered Survey Plans / Veris.
Adjoining information: DCDB.
Contours: Bradlees.

Legend

- Site Boundary
- Proposed Stage Boundary
- Proposed Sub Stage Boundary
- Existing Q100
- Possible Multiple Residential Allotment (Max. no. of dwellings)
- Entry Statements - Lease
- Indicative Indented Bus Stop Location
- Indicative Temporary In-Line Bus Stop Location
- Bus Stop Catchment (400m)
- Neighbourhood Park Catchment (400m)



CLIENT
PEET

PROJECT
Flagstone Precinct 1
Plan of Subdivision
Stage 4 Overall
Allotment Layout

| | |
|------------------|---------------------------------|
| Date: | 27 September 2024 |
| Comp By: | NF |
| Checked By: | MD |
| DWG Name: | Precinct 1 Stage 4 |
| Job Ref.: | 110056 |
| Local Authority: | Economic Development Queensland |
| Locality: | Flagstone |
| Scale: | 1 : 2000 |
| Sheet: | A1 |
| Plan Ref: | 110056 - 391 |
| Rev: | AM |

URBAN DESIGN
Level 102 South
520 Wickham Street
PO Box 1559
Fortitude Valley QLD 4006
T +61 7 3539 9500
W rpsgroup.com

© COPYRIGHT PROTECTS THIS PLAN
Unauthorized reproduction or amendment not permitted.
Please contact the author.

Yield Breakdown Stage 4

| Residential Allotments | Typical Size | Typical Area | Overall | |
|---|--------------|-------------------|------------|--------------|
| Urban & Nano Allotments Product | | | | |
| Urban Loft | 4.7 x 11.5m | 50m ² | — | 0.0% |
| Urban Allotments | 7.5 x 16m | 120m ² | — | 0.0% |
| Urban Terrace | 6.2 x 27.5m | 170m ² | — | 0.0% |
| Subtotal | | | — | 0.0% |
| 16m Deep Product | | | | |
| Squat Allotment | 14 x 16m | 220m ² | 4 | 0.8% |
| Subtotal | | | 4 | 0.8% |
| 25m Deep Product | | | | |
| Villa Allotment | 10 x 25m | 250m ² | 5 | 0.9% |
| Premium Villa Allotment | 12.5 x 25m | 313m ² | 3 | 0.6% |
| Courtyard Allotment | 14 x 25m | 350m ² | 28 | 5.3% |
| Premium Courtyard Allotment | 16 x 25m | 400m ² | 5 | 0.9% |
| Premium Traditional Allotment | 20 x 25m | 500m ² | — | 0.0% |
| Possible Multiple Residential Allotment | — | — | — | 0.0% |
| Subtotal | | | 41 | 7.8% |
| 28m Deep Product | | | | |
| Terrace 7.5m Allotment | 7.5 x 28m | 210m ² | 5 | 0.9% |
| Terrace 9.5m Allotment | 9.5 x 28m | 265m ² | 2 | 0.4% |
| Subtotal | | | 7 | 1.3% |
| 30m Deep Product | | | | |
| Villa Allotment | 10 x 30m | 300m ² | 111 | 21.0% |
| Premium Villa Allotment | 12.5 x 30m | 375m ² | 174 | 33.0% |
| Courtyard Allotment | 14 x 30m | 420m ² | 160 | 30.3% |
| Traditional Allotment | 20 x 30m | 600m ² | 26 | 4.9% |
| Premium Traditional Allotment | 25 x 30m | 720m ² | — | 0.0% |
| Possible Multiple Residential Allotment | — | — | — | 0.0% |
| Subtotal | | | 476 | 90.2% |
| Total Residential Allotments | | | 528 | 100% |

Land Budget Stage 4

| Land Use | Overall | |
|-------------------------------------|-------------------|---------------|
| | Area | % |
| Area of Subject Site | 301.996 ha | 100.0% |
| Saleable Area | | |
| Residential Allotments | 22.088 ha | 7.3% |
| Commercial Allotment | 1.324 ha | 0.4% |
| Balance Super Allotments | 246.968 ha | 81.8% |
| Community Facility | 0.500 ha | 0.2% |
| Total Area of Allotments | 270.880 ha | 89.7% |
| Road | | |
| Collector Road | 4.790 ha | 1.6% |
| Local Road | 9.331 ha | 3.1% |
| Linear Connections | 0.473 ha | 0.2% |
| Entry Statements | — | 0.0% |
| Total Area of New Road | 14.594 ha | 4.8% |
| Open Space | | |
| Corridor Park | 14.719 ha | 4.9% |
| Conservation (Within Corridor Park) | 6.357 ha | — |
| District Recreation Park | — | 0.0% |
| Neighbourhood Recreation Park | 1.803 ha | 0.6% |
| Local Recreation Park | — | 0.0% |
| Local Linear Recreation Park | — | 0.0% |
| Stormwater Detention | — | 0.0% |
| Total Open Space | 16.522 ha | 5.5% |

| Super Lots | Lots |
|---|-------------------|
| Commercial Allotment | 1 |
| Community Facility | 1 |
| Balance Super Allotments | 3 |
| Sub Total | 5 |
| Total Allotments | 533 |
| Maximum Potential Residential Dwellings (Includes Multiple Residential Allotments) | 535 |
| Maximum Potential Net Residential Density | 15.9 dw/ha |

Appendix B Schedule 2 of QDC MP4.4

Flagstone Development, Stage 4

Road Traffic Noise Intrusion Assessment

Peet Flagstone City Pty Ltd

SLR Project No.: 620.v10512.02006

25 October 2024



Table B-1 Schedule 2 of QDC MP4.4

| Component of Building's External Envelope | Minimum R_w | Acceptable Forms of Construction |
|---|---------------|---|
| Glazing | 43 | Double glazing consisting of two panes of minimum 5mm thick glass with at least 100mm air gap and full perimeter <i>acoustically rated seals</i> . |
| | 38 | Minimum 14.38mm thick laminated glass, with full perimeter <i>acoustically rated seals</i> ; or Double glazing consisting of one pane of minimum 5mm thick glass and one pane of minimum 6mm thick glass with at least 44mm air gap, and full perimeter <i>acoustically rated seals</i> |
| | 35 | Minimum 10.38mm thick laminated glass, with full perimeter <i>acoustically rated seals</i> . |
| | 32 | Minimum 6.38mm thick laminated glass with full perimeter <i>acoustically rated seals</i> . |
| | 27 | Minimum 4mm thick glass with full perimeter <i>acoustically rated seals</i> |
| | 24 | Minimum 4mm thick glass with standard weather seals |
| External Walls | 52 | Two leaves of clay brick masonry, at least 270mm in total, with subfloor vents fitted with noise attenuators. |
| | 47 | Two leaves of clay brick masonry at least 110mm thick with: (i) cavity not less than 50mm between leaves; and (ii) 50mm thick mineral insulation or 50mm thick glass wool insulation with a density of 11kg/m ³ or 50mm thick polyester insulation with a density of 20kg/m ³ in the cavity. or Two leaves of clay brick masonry at last 110mm thick with: (i) cavity not less than 50mm between leaves; and (ii) at least 13mm thick cement render on each face or Single leaf of clay brick masonry at least 110mm thick with: (i) a row of at least 70mm x 35mm timber studs or 64mm steel studs at 600mm centres, spaced at least 20mm from the masonry wall; and (ii) Mineral insulation or glass wool insulation at least 50mm thick with a density of at least 11 kg/m ³ positioned between studs; and (iii) One layer of plasterboard at least 13mm thick fixed to outside face of studs. or Single leaf of minimum 150mm thick masonry of hollow, dense concrete blocks, with mortar joints laid to prevent moisture bridging. |
| | 41 | Two leaves of clay brick masonry at least 110mm thick with cavity not less than 50mm between leaves |



| Component of Building's External Envelope | Minimum R_w | Acceptable Forms of Construction |
|---|---------------|---|
| | | or Single leaf of clay brick masonry at least 110mm thick with: (i) a row of at least 70mm x 35mm timber studs or 64mm steel studs at 600mm centres, spaced at least 20mm from the masonry wall; and (ii) mineral insulation or glass wool insulation at least 50mm thick with a density of at least 11 kg/m ³ positioned between studs; and (iii) One layer of plasterboard at least 10mm thick fixed to outside face of studs or Single leaf of brick masonry at least 110mm thick with at least 13mm thick render on each face or Concrete brickwork at least 110mm thick or In-situ concrete at least 100mm thick or Precast concrete at least 100mm thick and without joints. |
| | 35 | Single leaf of clay brick masonry at least 110mm thick with: (i) a row of at least 70mm x 35mm timber studs or 64mm steel studs at 600mm centres, spaced at least 20mm from the masonry wall; and (ii) One layer of plasterboard at least 10mm thick fixed to outside face of studs or Minimum 6mm thick fibre cement sheeting or weatherboards or plank cladding externally, minimum 90mm deep timber stud or 92mm metal stud, standard plasterboard at least 13mm thick internally. |
| Roof | 45 | Concrete or terracotta tile or sheet metal roof with sarking, <i>acoustically rated plasterboard</i> ceiling at least 13mm thick fixed to ceiling joists, cellulose fibre insulation at least 100mm thick with a density of at least 45kg/m ³ in the cavity. or Concrete or terracotta tile or sheet metal roof with sarking, 2 layers of <i>acoustically rated plasterboard</i> at least 16mm thick fixed to ceiling joists, glass wool insulation at least 50mm thick with a density of at least 11kg/m ³ or polyester insulation at least 50mm thick with a density of at least 20kg/m ³ in the cavity. |
| | 41 | Concrete or terracotta tile or metal sheet roof with sarking, plasterboard ceiling at least 10mm thick fixed to ceiling joists, glass wool insulation at least 50mm thick with a density of at least 11kg/m ³ or polyester insulation at least 50mm thick with a density of at least 20kg/m ³ in the cavity. or Concrete suspended slab at least 100mm thick. |



| Component of Building's External Envelope | Minimum R_w | Acceptable Forms of Construction |
|---|---------------|--|
| | 38 | Concrete or terracotta tile or metal sheet roof with sarking, plasterboard ceiling at least 10mm thick fixed to ceiling cavity, mineral insulation or glass wool insulation at least 50mm thick with a density of at least 11 kg/m ³ . |
| | 35 | Concrete or terracotta tile or metal sheet roof with sarking, plasterboard ceiling at least 10mm thick fixed to ceiling cavity. |
| Floors | 51 | Concrete slab at least 150mm thick. |
| | 45 | Concrete slab at least 100mm thick or Tongued and grooved boards at least 19mm thick with: (i) timber joists not less than 175mm x 50mm; and (ii) mineral insulation or glass wool insulation at least 75mm thick with a density of at least 11kg/m ³ positioned between joists and laid on plasterboard at least 10mm thick fixed to underside of joists; and (iii) mineral insulation or glass wool insulation at least 25mm thick with a density of at least 11kg/m ³ laid over entire floor, including tops of joists before flooring is laid; and (iv) secured to battens at least 75mm x 50mm; and (v) the assembled flooring laid over the joists, but not fixed to them, with battens lying between the joists. |



| Component of Building's External Envelope | Minimum R_w | Acceptable Forms of Construction |
|---|---------------|---|
| Entry Doors | 35 | Solid core timber not less than 45mm thick, fixed so as to overlap the frame or rebate of the frame by not less than 10mm, with full perimeter acoustically rated seals. |
| | 33 | Fixed so as to overlap the frame or rebate of the frame by not less than 10mm, fitted with full perimeter acoustically rated seals and constructed of - (i) solid core, wood, particleboard or blockboard not less than 45mm thick; and/or (ii) acoustically laminated glass not less than 10.38mm thick. |
| | 28 | Fixed so as to overlap the frame or rebate of the frame, constructed of - (i) Wood, particleboard or blockboard not less than 33mm thick; or (ii) Compressed fibre reinforced sheeting not less than 9mm thick; or (iii) Other suitable material with a mass per unit area not less than 24.4kg/m ² ; or (iv) Solid core timber door not less than 35mm thick fitted with full perimeter <i>acoustically rated seals</i> . |





Appendix C QDC MP4.4 Noise Predictions

Flagstone Development, Stage 4

Road Traffic Noise Intrusion Assessment

Peet Flagstone City Pty Ltd

SLR Project No.: 620.v10512.02006

25 October 2024

Table C-1 QDC MP4.4 noise predictions

| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1550 | GF | 495231.53 | 6926035.67 | 49.0 | 3 | 2 |
| 1550 | FF | 495231.53 | 6926035.67 | 49.0 | 3 | 3 |
| 1551 | GF | 495232.84 | 6926018.29 | 49.0 | 3 | 2 |
| 1551 | FF | 495232.84 | 6926018.29 | 49.0 | 3 | 3 |
| 1552 | GF | 495232.17 | 6926006.47 | 49.0 | 3 | 2 |
| 1552 | FF | 495232.17 | 6926006.47 | 49.0 | 3 | 3 |
| 1553 | GF | 495229.78 | 6925994.37 | 49.1 | 3 | 2 |
| 1553 | FF | 495229.78 | 6925994.37 | 49.1 | 3 | 3 |
| 1554 | GF | 495227.87 | 6925981.01 | 49.1 | 3 | 2 |
| 1554 | FF | 495227.87 | 6925981.01 | 49.1 | 3 | 3 |
| 1555 | GF | 495224.41 | 6925964.58 | 49.2 | 3 | 2 |
| 1555 | FF | 495224.41 | 6925964.58 | 49.2 | 3 | 3 |
| 1556 | GF | 495219.85 | 6925951.55 | 49.3 | 3 | 1 |
| 1556 | FF | 495219.85 | 6925951.55 | 49.3 | 3 | 3 |
| 1557 | GF | 495214.48 | 6925939 | 49.3 | 3 | 1 |
| 1557 | FF | 495214.48 | 6925939 | 49.3 | 3 | 3 |
| 1558 | GF | 495208.75 | 6925928.92 | 49.4 | 3 | 2 |
| 1558 | FF | 495208.75 | 6925928.92 | 49.4 | 3 | 3 |
| 1559 | GF | 495203.51 | 6925918.59 | 49.4 | 3 | 2 |
| 1559 | FF | 495203.51 | 6925918.59 | 49.4 | 3 | 3 |
| 1560 | GF | 495198.05 | 6925907.34 | 49.5 | 3 | 2 |
| 1560 | FF | 495198.05 | 6925907.34 | 49.5 | 3 | 3 |
| 1561 | GF | 495190.37 | 6925895.07 | 49.5 | 3 | 2 |
| 1561 | FF | 495190.37 | 6925895.07 | 49.5 | 3 | 3 |
| 1562 | GF | 494442.33 | 6925713.5 | 60.2 | 2 | 0 |
| 1562 | FF | 494442.33 | 6925713.5 | 60.2 | 3 | 2 |
| 1563 | GF | 495149.34 | 6925854.92 | 49.8 | 3 | 1 |
| 1563 | FF | 495149.34 | 6925854.92 | 49.8 | 3 | 3 |
| 1564 | GF | 495138.58 | 6925847.84 | 49.9 | 3 | 2 |
| 1564 | FF | 495138.58 | 6925847.84 | 49.9 | 3 | 3 |
| 1565 | GF | 495128.96 | 6925842.34 | 49.9 | 3 | 2 |
| 1565 | FF | 495128.96 | 6925842.34 | 49.9 | 3 | 3 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1566 | GF | 495117.9 | 6925837.12 | 49.9 | 3 | 1 |
| 1566 | FF | 495117.9 | 6925837.12 | 49.9 | 3 | 3 |
| 1567 | GF | 495105.72 | 6925830.71 | 49.9 | 3 | 2 |
| 1567 | FF | 495105.72 | 6925830.71 | 49.9 | 3 | 3 |
| 1568 | GF | 495094.73 | 6925826.98 | 50.0 | 3 | 1 |
| 1568 | FF | 495094.73 | 6925826.98 | 50.0 | 3 | 3 |
| 1569 | GF | 495084.77 | 6925823.36 | 50.3 | 3 | 2 |
| 1569 | FF | 495084.77 | 6925823.36 | 50.3 | 3 | 3 |
| 1570 | GF | 495073.01 | 6925819.2 | 50.4 | 3 | 2 |
| 1570 | FF | 495073.01 | 6925819.2 | 50.4 | 3 | 3 |
| 1571 | GF | 495057.68 | 6925815.84 | 51.3 | 3 | 2 |
| 1571 | FF | 495057.68 | 6925815.84 | 51.3 | 3 | 3 |
| 1572 | GF | 495043.66 | 6925813.56 | 51.4 | 3 | 2 |
| 1572 | FF | 495043.66 | 6925813.56 | 51.4 | 3 | 3 |
| 1573 | GF | 495031.53 | 6925813.03 | 51.4 | 3 | 1 |
| 1573 | FF | 495031.53 | 6925813.03 | 51.4 | 3 | 3 |
| 1574 | GF | 495020.17 | 6925812.49 | 51.2 | 2 | 1 |
| 1574 | FF | 495020.17 | 6925812.49 | 51.2 | 3 | 3 |
| 1575 | GF | 495005.67 | 6925810.96 | 51.0 | 2 | 1 |
| 1575 | FF | 495005.67 | 6925810.96 | 51.0 | 3 | 3 |
| 1576 | GF | 495033.15 | 6925861.32 | 51.4 | 1 | 0 |
| 1576 | FF | 495033.15 | 6925861.32 | 51.4 | 1 | 1 |
| 1577 | GF | 495047.58 | 6925864.07 | 51.5 | 1 | 0 |
| 1577 | FF | 495047.58 | 6925864.07 | 51.5 | 1 | 1 |
| 1578 | GF | 495061.66 | 6925867.7 | 51.5 | 1 | 0 |
| 1578 | FF | 495061.66 | 6925867.7 | 51.5 | 1 | 1 |
| 1579 | GF | 495077.1 | 6925872.17 | 51.4 | 1 | 0 |
| 1579 | FF | 495077.1 | 6925872.17 | 51.4 | 1 | 1 |
| 1580 | GF | 495088.71 | 6925875.61 | 51.3 | 1 | 0 |
| 1580 | FF | 495088.71 | 6925875.61 | 51.3 | 1 | 1 |
| 1581 | GF | 495100.56 | 6925884.64 | 51.2 | 1 | 0 |
| 1581 | FF | 495100.56 | 6925884.64 | 51.2 | 1 | 1 |
| 1582 | GF | 495113.14 | 6925892.56 | 51.1 | 1 | 0 |
| 1582 | FF | 495113.14 | 6925892.56 | 51.1 | 1 | 1 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1583 | GF | 495161.59 | 6925942.08 | 50.7 | 1 | 0 |
| 1583 | FF | 495161.59 | 6925942.08 | 50.7 | 1 | 1 |
| 1584 | GF | 495166.58 | 6925954.06 | 50.7 | 1 | 0 |
| 1584 | FF | 495166.58 | 6925954.06 | 50.7 | 1 | 1 |
| 1585 | GF | 495171.98 | 6925966.82 | 50.6 | 1 | 0 |
| 1585 | FF | 495171.98 | 6925966.82 | 50.6 | 1 | 1 |
| 1586 | GF | 495176.71 | 6925979.22 | 50.6 | 1 | 0 |
| 1586 | FF | 495176.71 | 6925979.22 | 50.6 | 1 | 1 |
| 1587 | GF | 495179.2 | 6925990.57 | 50.4 | 1 | 1 |
| 1587 | FF | 495179.2 | 6925990.57 | 50.4 | 1 | 1 |
| 1588 | GF | 495182.7 | 6926003.17 | 50.4 | 1 | 1 |
| 1588 | FF | 495182.7 | 6926003.17 | 50.4 | 1 | 1 |
| 1589 | GF | 495181.88 | 6926020.12 | 50.4 | 1 | 1 |
| 1589 | FF | 495181.88 | 6926020.12 | 50.4 | 1 | 1 |
| 1590 | GF | 495152.22 | 6926016.13 | 50.6 | 1 | 1 |
| 1590 | FF | 495152.22 | 6926016.13 | 50.6 | 1 | 1 |
| 1591 | GF | 495150.63 | 6925999.09 | 50.8 | 1 | 0 |
| 1591 | FF | 495150.63 | 6925999.09 | 50.8 | 1 | 1 |
| 1592 | GF | 495146.91 | 6925986.04 | 51.1 | 1 | 0 |
| 1592 | FF | 495146.91 | 6925986.04 | 51.1 | 1 | 1 |
| 1593 | GF | 495141.3 | 6925973.31 | 51.3 | 1 | 0 |
| 1593 | FF | 495141.3 | 6925973.31 | 51.3 | 1 | 1 |
| 1594 | GF | 495136.41 | 6925961.24 | 51.6 | 1 | 0 |
| 1594 | FF | 495136.41 | 6925961.24 | 51.6 | 1 | 1 |
| 1595 | GF | 495129.59 | 6925951.61 | 51.8 | 1 | 0 |
| 1595 | FF | 495129.59 | 6925951.61 | 51.8 | 1 | 1 |
| 1596 | GF | 495094.06 | 6925915 | 52.8 | 1 | 0 |
| 1596 | FF | 495094.06 | 6925915 | 52.8 | 1 | 1 |
| 1597 | GF | 495079.91 | 6925904.61 | 53.1 | 1 | 0 |
| 1597 | FF | 495079.91 | 6925904.61 | 53.1 | 1 | 1 |
| 1598 | GF | 494967.73 | 6925887.39 | 51.8 | 0 | 0 |
| 1598 | FF | 494967.73 | 6925887.39 | 51.8 | 1 | 0 |
| 1599 | GF | 494963.94 | 6925856.84 | 51.5 | 1 | 0 |
| 1599 | FF | 494963.94 | 6925856.84 | 51.5 | 1 | 1 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1600 | GF | 494963.71 | 6925844.86 | 51.4 | 1 | 1 |
| 1600 | FF | 494963.71 | 6925844.86 | 51.4 | 1 | 1 |
| 1601 | GF | 494971.22 | 6925805.66 | 50.6 | 2 | 2 |
| 1601 | FF | 494971.22 | 6925805.66 | 50.6 | 2 | 2 |
| 1602 | GF | 494957.6 | 6925800.57 | 50.6 | 2 | 2 |
| 1602 | FF | 494957.6 | 6925800.57 | 50.6 | 2 | 2 |
| 1603 | GF | 494944.98 | 6925794.57 | 50.5 | 2 | 2 |
| 1603 | FF | 494944.98 | 6925794.57 | 50.5 | 2 | 2 |
| 1604 | GF | 494935.25 | 6925830.44 | 50.0 | 1 | 1 |
| 1604 | FF | 494935.25 | 6925830.44 | 50.0 | 1 | 1 |
| 1605 | GF | 494934.22 | 6925844.05 | 49.9 | 0 | 0 |
| 1605 | FF | 494934.22 | 6925844.05 | 49.9 | 1 | 1 |
| 1606 | GF | 494933.31 | 6925855.37 | 49.8 | 0 | 0 |
| 1606 | FF | 494933.31 | 6925855.37 | 49.8 | 1 | 1 |
| 1607 | GF | 494939.43 | 6925886.17 | 50.9 | 0 | 0 |
| 1607 | FF | 494939.43 | 6925886.17 | 50.9 | 1 | 0 |
| 1608 | GF | 494926.74 | 6925884.83 | 50.3 | 0 | 0 |
| 1608 | FF | 494926.74 | 6925884.83 | 50.3 | 0 | 0 |
| 1609 | GF | 494888.55 | 6925812.12 | 49.9 | 1 | 0 |
| 1609 | FF | 494888.55 | 6925812.12 | 49.9 | 1 | 1 |
| 1610 | GF | 494888.59 | 6925825.57 | 49.8 | 0 | 0 |
| 1610 | FF | 494888.59 | 6925825.57 | 49.8 | 1 | 1 |
| 1611 | GF | 494887.91 | 6925838.59 | 49.7 | 0 | 0 |
| 1611 | FF | 494887.91 | 6925838.59 | 49.7 | 1 | 0 |
| 1612 | GF | 494887.07 | 6925850.13 | 49.7 | 0 | 0 |
| 1612 | FF | 494887.07 | 6925850.13 | 49.7 | 1 | 0 |
| 1613 | GF | 494882.75 | 6925920.61 | 49.2 | 0 | 0 |
| 1613 | FF | 494882.75 | 6925920.61 | 49.2 | 0 | 0 |
| 1615 | GF | 494929.9 | 6925927.97 | 49.6 | 0 | 0 |
| 1615 | FF | 494929.9 | 6925927.97 | 49.6 | 0 | 0 |
| 1616 | GF | 494929.28 | 6925938.85 | 49.9 | 0 | 0 |
| 1616 | FF | 494929.28 | 6925938.85 | 49.9 | 0 | 0 |
| 1617 | GF | 494928.52 | 6925950.98 | 49.8 | 0 | 0 |
| 1617 | FF | 494928.52 | 6925950.98 | 49.8 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1618 | GF | 494919.05 | 6925981.52 | 50.0 | 0 | 0 |
| 1618 | FF | 494919.05 | 6925981.52 | 50.0 | 0 | 0 |
| 1619 | GF | 494934.17 | 6925983.46 | 50.2 | 0 | 0 |
| 1619 | FF | 494934.17 | 6925983.46 | 50.2 | 0 | 0 |
| 1620 | GF | 494948.01 | 6925984.26 | 50.3 | 0 | 0 |
| 1620 | FF | 494948.01 | 6925984.26 | 50.3 | 0 | 0 |
| 1621 | GF | 494961.66 | 6925984.23 | 50.4 | 0 | 0 |
| 1621 | FF | 494961.66 | 6925984.23 | 50.4 | 0 | 0 |
| 1622 | GF | 494958.19 | 6925952.24 | 51.9 | 0 | 0 |
| 1622 | FF | 494958.19 | 6925952.24 | 51.9 | 0 | 0 |
| 1623 | GF | 494958.35 | 6925940.4 | 52.0 | 0 | 0 |
| 1623 | FF | 494958.35 | 6925940.4 | 52.0 | 0 | 0 |
| 1624 | GF | 494958.51 | 6925930.46 | 51.8 | 0 | 0 |
| 1624 | FF | 494958.51 | 6925930.46 | 51.8 | 0 | 0 |
| 1625 | GF | 495005.44 | 6925941.8 | 52.3 | 0 | 0 |
| 1625 | FF | 495005.44 | 6925941.8 | 52.3 | 0 | 0 |
| 1626 | GF | 495005.22 | 6925951.09 | 52.2 | 0 | 0 |
| 1626 | FF | 495005.22 | 6925951.09 | 52.2 | 0 | 0 |
| 1627 | GF | 495004.83 | 6925964.15 | 52.0 | 0 | 0 |
| 1627 | FF | 495004.83 | 6925964.15 | 52.0 | 0 | 0 |
| 1628 | GF | 495003.45 | 6925975.48 | 51.8 | 0 | 0 |
| 1628 | FF | 495003.45 | 6925975.48 | 51.8 | 0 | 0 |
| 1629 | GF | 495003.06 | 6925986.97 | 51.6 | 0 | 0 |
| 1629 | FF | 495003.06 | 6925986.97 | 51.6 | 0 | 0 |
| 1630 | GF | 495002.49 | 6925999.43 | 51.2 | 0 | 0 |
| 1630 | FF | 495002.49 | 6925999.43 | 51.2 | 0 | 0 |
| 1631 | GF | 495001.09 | 6926012.36 | 50.8 | 0 | 0 |
| 1631 | FF | 495001.09 | 6926012.36 | 50.8 | 0 | 0 |
| 1632 | GF | 494992.63 | 6926033.31 | 50.0 | 0 | 0 |
| 1632 | FF | 494992.63 | 6926033.31 | 50.0 | 0 | 0 |
| 1633 | GF | 495007.09 | 6926034.88 | 49.7 | 0 | 0 |
| 1633 | FF | 495007.09 | 6926034.88 | 49.7 | 0 | 0 |
| 1634 | GF | 495020.66 | 6926035.46 | 49.5 | 0 | 0 |
| 1634 | FF | 495020.66 | 6926035.46 | 49.5 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1635 | GF | 495035.62 | 6926035.57 | 49.3 | 0 | 0 |
| 1635 | FF | 495035.62 | 6926035.57 | 49.3 | 0 | 0 |
| 1636 | GF | 495029.71 | 6926013.62 | 50.5 | 0 | 0 |
| 1636 | FF | 495029.71 | 6926013.62 | 50.5 | 0 | 0 |
| 1637 | GF | 495030.74 | 6926001.02 | 51.5 | 0 | 0 |
| 1637 | FF | 495030.74 | 6926001.02 | 51.5 | 0 | 0 |
| 1638 | GF | 495031.83 | 6925988.35 | 52.0 | 0 | 0 |
| 1638 | FF | 495031.83 | 6925988.35 | 52.0 | 0 | 0 |
| 1639 | GF | 495031.02 | 6925977.15 | 52.6 | 0 | 0 |
| 1639 | FF | 495031.02 | 6925977.15 | 52.6 | 0 | 0 |
| 1640 | GF | 494739.54 | 6925923.49 | 49.1 | 1 | 1 |
| 1640 | FF | 494739.54 | 6925923.49 | 49.1 | 1 | 1 |
| 1641 | GF | 494746.37 | 6925909.73 | 49.3 | 0 | 0 |
| 1641 | FF | 494746.37 | 6925909.73 | 49.3 | 1 | 1 |
| 1642 | GF | 494751.27 | 6925895.71 | 48.8 | 1 | 0 |
| 1642 | FF | 494751.27 | 6925895.71 | 48.8 | 1 | 1 |
| 1643 | GF | 494757.98 | 6925881 | 48.7 | 1 | 1 |
| 1643 | FF | 494757.98 | 6925881 | 48.7 | 1 | 1 |
| 1644 | GF | 494763.61 | 6925847.87 | 49.2 | 1 | 1 |
| 1644 | FF | 494763.61 | 6925847.87 | 49.2 | 1 | 1 |
| 1645 | GF | 494768.53 | 6925834.52 | 49.4 | 1 | 1 |
| 1645 | FF | 494768.53 | 6925834.52 | 49.4 | 1 | 1 |
| 1646 | GF | 494776.6 | 6925807.24 | 49.8 | 1 | 1 |
| 1646 | FF | 494776.6 | 6925807.24 | 49.8 | 1 | 1 |
| 1647 | GF | 494779.5 | 6925796.99 | 49.8 | 1 | 1 |
| 1647 | FF | 494779.5 | 6925796.99 | 49.8 | 1 | 1 |
| 1648 | GF | 494782.99 | 6925784.28 | 49.9 | 1 | 1 |
| 1648 | FF | 494782.99 | 6925784.28 | 49.9 | 1 | 1 |
| 1649 | GF | 494796.17 | 6925767.79 | 50.0 | 1 | 1 |
| 1649 | FF | 494796.17 | 6925767.79 | 50.0 | 1 | 1 |
| 1650 | GF | 494812.71 | 6925755.44 | 50.1 | 1 | 1 |
| 1650 | FF | 494812.71 | 6925755.44 | 50.1 | 1 | 1 |
| 1651 | GF | 494823.24 | 6925747.4 | 50.3 | 1 | 1 |
| 1651 | FF | 494823.24 | 6925747.4 | 50.3 | 2 | 1 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1652 | GF | 494833.77 | 6925739.37 | 50.8 | 1 | 1 |
| 1652 | FF | 494833.77 | 6925739.37 | 50.8 | 2 | 2 |
| 1653 | GF | 494844.1 | 6925731.49 | 51.3 | 2 | 1 |
| 1653 | FF | 494844.1 | 6925731.49 | 51.3 | 2 | 2 |
| 1654 | GF | 494874.19 | 6925708.2 | 52.0 | 3 | 2 |
| 1654 | FF | 494874.19 | 6925708.2 | 52.0 | 3 | 3 |
| 1655 | GF | 494856.23 | 6925693.3 | 52.2 | 3 | 1 |
| 1655 | FF | 494856.23 | 6925693.3 | 52.2 | 3 | 3 |
| 1656 | GF | 494844.59 | 6925677.3 | 52.5 | 3 | 1 |
| 1656 | FF | 494844.59 | 6925677.3 | 52.5 | 3 | 3 |
| 1657 | GF | 494825.91 | 6925709.97 | 52.4 | 1 | 1 |
| 1657 | FF | 494825.91 | 6925709.97 | 52.4 | 2 | 1 |
| 1658 | GF | 494815.58 | 6925716.94 | 52.3 | 1 | 1 |
| 1658 | FF | 494815.58 | 6925716.94 | 52.3 | 1 | 1 |
| 1659 | GF | 494805.05 | 6925724.97 | 52.2 | 1 | 1 |
| 1659 | FF | 494805.05 | 6925724.97 | 52.2 | 1 | 1 |
| 1660 | GF | 494794.51 | 6925733 | 52.2 | 1 | 1 |
| 1660 | FF | 494794.51 | 6925733 | 52.2 | 1 | 1 |
| 1661 | GF | 494784.58 | 6925740.58 | 52.1 | 1 | 0 |
| 1661 | FF | 494784.58 | 6925740.58 | 52.1 | 1 | 1 |
| 1662 | GF | 494774.64 | 6925748.16 | 52.0 | 1 | 0 |
| 1662 | FF | 494774.64 | 6925748.16 | 52.0 | 1 | 1 |
| 1663 | GF | 494762.22 | 6925759.82 | 51.9 | 0 | 0 |
| 1663 | FF | 494762.22 | 6925759.82 | 51.9 | 1 | 0 |
| 1664 | GF | 494757.7 | 6925776.68 | 51.8 | 0 | 0 |
| 1664 | FF | 494757.7 | 6925776.68 | 51.8 | 1 | 0 |
| 1665 | GF | 494753.83 | 6925789.45 | 51.7 | 0 | 0 |
| 1665 | FF | 494753.83 | 6925789.45 | 51.7 | 1 | 0 |
| 1666 | GF | 494748.38 | 6925802.41 | 51.5 | 0 | 0 |
| 1666 | FF | 494748.38 | 6925802.41 | 51.5 | 1 | 0 |
| 1667 | GF | 494743.31 | 6925835.75 | 49.9 | 0 | 0 |
| 1667 | FF | 494743.31 | 6925835.75 | 49.9 | 0 | 0 |
| 1668 | GF | 494731.82 | 6925829.36 | 50.6 | 0 | 0 |
| 1668 | FF | 494731.82 | 6925829.36 | 50.6 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1669 | GF | 494700.2 | 6925822.32 | 51.6 | 0 | 0 |
| 1669 | FF | 494700.2 | 6925822.32 | 51.6 | 0 | 0 |
| 1670 | GF | 494684.27 | 6925817.6 | 52.3 | 0 | 0 |
| 1670 | FF | 494684.27 | 6925817.6 | 52.3 | 0 | 0 |
| 1671 | GF | 494703.44 | 6925782.56 | 52.0 | 0 | 0 |
| 1671 | FF | 494703.44 | 6925782.56 | 52.0 | 0 | 0 |
| 1672 | GF | 494708.6 | 6925770.09 | 52.1 | 0 | 0 |
| 1672 | FF | 494708.6 | 6925770.09 | 52.1 | 0 | 0 |
| 1673 | GF | 494712.02 | 6925758.65 | 52.1 | 0 | 0 |
| 1673 | FF | 494712.02 | 6925758.65 | 52.1 | 0 | 0 |
| 1674 | GF | 494716.52 | 6925745.6 | 52.2 | 0 | 0 |
| 1674 | FF | 494716.52 | 6925745.6 | 52.2 | 0 | 0 |
| 1675 | GF | 494723.2 | 6925730.36 | 52.3 | 0 | 0 |
| 1675 | FF | 494723.2 | 6925730.36 | 52.3 | 0 | 0 |
| 1676 | GF | 494759.81 | 6925702.87 | 52.1 | 0 | 0 |
| 1676 | FF | 494759.81 | 6925702.87 | 52.1 | 1 | 0 |
| 1677 | GF | 494770.12 | 6925695.27 | 52.2 | 0 | 0 |
| 1677 | FF | 494770.12 | 6925695.27 | 52.2 | 1 | 1 |
| 1678 | GF | 494780.85 | 6925686.64 | 52.4 | 1 | 0 |
| 1678 | FF | 494780.85 | 6925686.64 | 52.4 | 1 | 1 |
| 1679 | GF | 494831.82 | 6925660.73 | 52.8 | 2 | 1 |
| 1679 | FF | 494831.82 | 6925660.73 | 52.8 | 3 | 3 |
| 1680 | GF | 494823.4 | 6925650.49 | 53.0 | 2 | 1 |
| 1680 | FF | 494823.4 | 6925650.49 | 53.0 | 3 | 3 |
| 1681 | GF | 494816.58 | 6925641.55 | 53.4 | 2 | 1 |
| 1681 | FF | 494816.58 | 6925641.55 | 53.4 | 3 | 3 |
| 1682 | GF | 494810.51 | 6925633.6 | 53.7 | 2 | 1 |
| 1682 | FF | 494810.51 | 6925633.6 | 53.7 | 3 | 3 |
| 1683 | GF | 494803.28 | 6925624.24 | 54.2 | 2 | 2 |
| 1683 | FF | 494803.28 | 6925624.24 | 54.2 | 3 | 3 |
| 1684 | GF | 494796.45 | 6925613.89 | 54.8 | 2 | 2 |
| 1684 | FF | 494796.45 | 6925613.89 | 54.8 | 3 | 3 |
| 1685 | GF | 494789.3 | 6925605.78 | 55.3 | 2 | 2 |
| 1685 | FF | 494789.3 | 6925605.78 | 55.3 | 3 | 3 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1686 | GF | 494783.3 | 6925597.59 | 55.8 | 2 | 2 |
| 1686 | FF | 494783.3 | 6925597.59 | 55.8 | 3 | 3 |
| 1687 | GF | 494776.31 | 6925588.51 | 56.3 | 2 | 2 |
| 1687 | FF | 494776.31 | 6925588.51 | 56.3 | 3 | 3 |
| 1688 | GF | 494769.2 | 6925578.84 | 57.0 | 3 | 2 |
| 1688 | FF | 494769.2 | 6925578.84 | 57.0 | 3 | 3 |
| 1689 | GF | 494760.54 | 6925567.93 | 57.6 | 3 | 2 |
| 1689 | FF | 494760.54 | 6925567.93 | 57.6 | 3 | 3 |
| 1690 | GF | 494753.53 | 6925557.37 | 58.0 | 3 | 2 |
| 1690 | FF | 494753.53 | 6925557.37 | 58.0 | 3 | 3 |
| 1691 | GF | 494707.8 | 6925523.35 | 59.4 | 3 | 2 |
| 1691 | FF | 494707.8 | 6925523.35 | 59.4 | 3 | 3 |
| 1692 | GF | 494693.07 | 6925536.29 | 59.4 | 2 | 1 |
| 1692 | FF | 494693.07 | 6925536.29 | 59.4 | 2 | 2 |
| 1693 | GF | 494682.53 | 6925544.32 | 59.1 | 1 | 1 |
| 1693 | FF | 494682.53 | 6925544.32 | 59.1 | 2 | 2 |
| 1694 | GF | 494672.6 | 6925551.9 | 59.1 | 1 | 1 |
| 1694 | FF | 494672.6 | 6925551.9 | 59.1 | 2 | 2 |
| 1695 | GF | 494663.85 | 6925559.03 | 59.2 | 1 | 1 |
| 1695 | FF | 494663.85 | 6925559.03 | 59.2 | 2 | 2 |
| 1696 | GF | 494654.91 | 6925565.09 | 59.5 | 2 | 1 |
| 1696 | FF | 494654.91 | 6925565.09 | 59.5 | 3 | 3 |
| 1697 | GF | 494716.64 | 6925586.43 | 58.0 | 1 | 1 |
| 1697 | FF | 494716.64 | 6925586.43 | 58.0 | 2 | 1 |
| 1698 | GF | 494724.73 | 6925596.18 | 57.5 | 1 | 1 |
| 1698 | FF | 494724.73 | 6925596.18 | 57.5 | 1 | 1 |
| 1699 | GF | 494731.62 | 6925607.13 | 56.9 | 1 | 1 |
| 1699 | FF | 494731.62 | 6925607.13 | 56.9 | 1 | 1 |
| 1700 | GF | 494739.5 | 6925617.16 | 56.3 | 1 | 0 |
| 1700 | FF | 494739.5 | 6925617.16 | 56.3 | 1 | 1 |
| 1701 | GF | 494751.27 | 6925630.94 | 55.2 | 1 | 0 |
| 1701 | FF | 494751.27 | 6925630.94 | 55.2 | 1 | 1 |
| 1702 | GF | 494761.07 | 6925645.23 | 54.6 | 1 | 0 |
| 1702 | FF | 494761.07 | 6925645.23 | 54.6 | 1 | 1 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1703 | GF | 494769.02 | 6925654.78 | 54.1 | 1 | 0 |
| 1703 | FF | 494769.02 | 6925654.78 | 54.1 | 1 | 1 |
| 1704 | GF | 494776.32 | 6925664.98 | 53.5 | 1 | 0 |
| 1704 | FF | 494776.32 | 6925664.98 | 53.5 | 1 | 1 |
| 1705 | GF | 494753.85 | 6925681.08 | 52.8 | 0 | 0 |
| 1705 | FF | 494753.85 | 6925681.08 | 52.8 | 1 | 1 |
| 1706 | GF | 494746.21 | 6925671.48 | 53.4 | 0 | 0 |
| 1706 | FF | 494746.21 | 6925671.48 | 53.4 | 1 | 1 |
| 1707 | GF | 494738.24 | 6925661.65 | 54.0 | 0 | 0 |
| 1707 | FF | 494738.24 | 6925661.65 | 54.0 | 1 | 1 |
| 1708 | GF | 494727.45 | 6925647.13 | 54.6 | 0 | 0 |
| 1708 | FF | 494727.45 | 6925647.13 | 54.6 | 1 | 1 |
| 1709 | GF | 494717.12 | 6925633.14 | 56.0 | 1 | 0 |
| 1709 | FF | 494717.12 | 6925633.14 | 56.0 | 1 | 1 |
| 1710 | GF | 494707.63 | 6925623.96 | 56.6 | 1 | 0 |
| 1710 | FF | 494707.63 | 6925623.96 | 56.6 | 1 | 1 |
| 1711 | GF | 494701.63 | 6925612.81 | 57.2 | 1 | 0 |
| 1711 | FF | 494701.63 | 6925612.81 | 57.2 | 1 | 1 |
| 1712 | GF | 494694.41 | 6925604.2 | 57.7 | 1 | 0 |
| 1712 | FF | 494694.41 | 6925604.2 | 57.7 | 1 | 1 |
| 1713 | GF | 494711.02 | 6925708.17 | 53.0 | 0 | 0 |
| 1713 | FF | 494711.02 | 6925708.17 | 53.0 | 0 | 0 |
| 1714 | GF | 494701.7 | 6925693.32 | 54.0 | 0 | 0 |
| 1714 | FF | 494701.7 | 6925693.32 | 54.0 | 0 | 0 |
| 1715 | GF | 494695.09 | 6925681.75 | 54.8 | 0 | 0 |
| 1715 | FF | 494695.09 | 6925681.75 | 54.8 | 1 | 0 |
| 1716 | GF | 494686.81 | 6925673.18 | 55.4 | 0 | 0 |
| 1716 | FF | 494686.81 | 6925673.18 | 55.4 | 1 | 0 |
| 1717 | GF | 494680.46 | 6925664.82 | 55.9 | 0 | 0 |
| 1717 | FF | 494680.46 | 6925664.82 | 55.9 | 1 | 0 |
| 1718 | GF | 494672.71 | 6925654.1 | 56.5 | 0 | 0 |
| 1718 | FF | 494672.71 | 6925654.1 | 56.5 | 1 | 0 |
| 1719 | GF | 494663.97 | 6925643.5 | 57.3 | 0 | 0 |
| 1719 | FF | 494663.97 | 6925643.5 | 57.3 | 1 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1720 | GF | 494640.58 | 6925601.19 | 60.3 | 2 | 1 |
| 1720 | FF | 494640.58 | 6925601.19 | 60.3 | 2 | 2 |
| 1721 | GF | 494630.64 | 6925607.56 | 61.3 | 2 | 2 |
| 1721 | FF | 494630.64 | 6925607.56 | 61.3 | 2 | 2 |
| 1722 | GF | 494622.7 | 6925613.62 | 61.5 | 2 | 2 |
| 1722 | FF | 494622.7 | 6925613.62 | 61.5 | 2 | 2 |
| 1723 | GF | 494614.75 | 6925619.68 | 61.5 | 2 | 2 |
| 1723 | FF | 494614.75 | 6925619.68 | 61.5 | 2 | 2 |
| 1724 | GF | 494606.02 | 6925627.26 | 60.5 | 2 | 1 |
| 1724 | FF | 494606.02 | 6925627.26 | 60.5 | 2 | 2 |
| 1725 | GF | 494638.82 | 6925660.48 | 58.0 | 0 | 0 |
| 1725 | FF | 494638.82 | 6925660.48 | 58.0 | 1 | 0 |
| 1726 | GF | 494647.46 | 6925672.26 | 57.1 | 0 | 0 |
| 1726 | FF | 494647.46 | 6925672.26 | 57.1 | 1 | 0 |
| 1727 | GF | 494656.24 | 6925681.55 | 56.6 | 0 | 0 |
| 1727 | FF | 494656.24 | 6925681.55 | 56.6 | 1 | 0 |
| 1728 | GF | 494662.17 | 6925689.71 | 56.1 | 0 | 0 |
| 1728 | FF | 494662.17 | 6925689.71 | 56.1 | 1 | 0 |
| 1729 | GF | 494670.36 | 6925698.1 | 55.5 | 0 | 0 |
| 1729 | FF | 494670.36 | 6925698.1 | 55.5 | 0 | 0 |
| 1730 | GF | 494678.56 | 6925709.32 | 55.1 | 0 | 0 |
| 1730 | FF | 494678.56 | 6925709.32 | 55.1 | 0 | 0 |
| 1731 | GF | 494687.73 | 6925732.35 | 54.5 | 0 | 0 |
| 1731 | FF | 494687.73 | 6925732.35 | 54.5 | 0 | 0 |
| 1732 | GF | 494683.87 | 6925748.26 | 54.3 | 0 | 0 |
| 1732 | FF | 494683.87 | 6925748.26 | 54.3 | 0 | 0 |
| 1733 | GF | 494680.72 | 6925759.62 | 54.1 | 0 | 0 |
| 1733 | FF | 494680.72 | 6925759.62 | 54.1 | 0 | 0 |
| 1734 | GF | 494676.83 | 6925772.66 | 54.0 | 0 | 0 |
| 1734 | FF | 494676.83 | 6925772.66 | 54.0 | 0 | 0 |
| 1735 | GF | 494670.83 | 6925812.89 | 52.8 | 0 | 0 |
| 1735 | FF | 494670.83 | 6925812.89 | 52.8 | 0 | 0 |
| 1736 | GF | 494792.87 | 6925678.42 | 52.6 | 1 | 0 |
| 1736 | FF | 494792.87 | 6925678.42 | 52.6 | 1 | 1 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1737 | GF | 494642.01 | 6925851.63 | 52.1 | 0 | 0 |
| 1737 | FF | 494642.01 | 6925851.63 | 52.1 | 0 | 0 |
| 1738 | GF | 494656.41 | 6925855.81 | 52.5 | 0 | 0 |
| 1738 | FF | 494656.41 | 6925855.81 | 52.5 | 0 | 0 |
| 1739 | GF | 494673.9 | 6925862.9 | 52.2 | 0 | 0 |
| 1739 | FF | 494673.9 | 6925862.9 | 52.2 | 0 | 0 |
| 1740 | GF | 494687.14 | 6925868.02 | 51.8 | 0 | 0 |
| 1740 | FF | 494687.14 | 6925868.02 | 51.8 | 0 | 0 |
| 1741 | GF | 494699.09 | 6925871.24 | 51.2 | 0 | 0 |
| 1741 | FF | 494699.09 | 6925871.24 | 51.2 | 0 | 0 |
| 1742 | GF | 494711.11 | 6925876 | 50.6 | 0 | 0 |
| 1742 | FF | 494711.11 | 6925876 | 50.6 | 0 | 0 |
| 1743 | GF | 494702.76 | 6925901.11 | 50.4 | 0 | 0 |
| 1743 | FF | 494702.76 | 6925901.11 | 50.4 | 1 | 0 |
| 1744 | GF | 494689.99 | 6925897.44 | 50.7 | 0 | 0 |
| 1744 | FF | 494689.99 | 6925897.44 | 50.7 | 1 | 0 |
| 1745 | GF | 494678.92 | 6925893.04 | 51.0 | 0 | 0 |
| 1745 | FF | 494678.92 | 6925893.04 | 51.0 | 1 | 0 |
| 1746 | GF | 494665.13 | 6925888.58 | 51.2 | 0 | 0 |
| 1746 | FF | 494665.13 | 6925888.58 | 51.2 | 1 | 0 |
| 1747 | GF | 494649.03 | 6925881.96 | 51.5 | 0 | 0 |
| 1747 | FF | 494649.03 | 6925881.96 | 51.5 | 1 | 0 |
| 1748 | GF | 494633.79 | 6925876.76 | 51.5 | 0 | 0 |
| 1748 | FF | 494633.79 | 6925876.76 | 51.5 | 1 | 0 |
| 1749 | GF | 494603.93 | 6925864.84 | 51.7 | 0 | 0 |
| 1749 | FF | 494603.93 | 6925864.84 | 51.7 | 1 | 0 |
| 1750 | GF | 494588.58 | 6925860.99 | 52.2 | 0 | 0 |
| 1750 | FF | 494588.58 | 6925860.99 | 52.2 | 1 | 0 |
| 1751 | GF | 494606.92 | 6925824.88 | 53.3 | 0 | 0 |
| 1751 | FF | 494606.92 | 6925824.88 | 53.3 | 0 | 0 |
| 1752 | GF | 494610.5 | 6925813.72 | 53.7 | 0 | 0 |
| 1752 | FF | 494610.5 | 6925813.72 | 53.7 | 0 | 0 |
| 1753 | GF | 494614.64 | 6925804.99 | 53.9 | 0 | 0 |
| 1753 | FF | 494614.64 | 6925804.99 | 53.9 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1754 | GF | 494618.75 | 6925793.55 | 54.1 | 0 | 0 |
| 1754 | FF | 494618.75 | 6925793.55 | 54.1 | 0 | 0 |
| 1755 | GF | 494623.1 | 6925781.04 | 54.2 | 0 | 0 |
| 1755 | FF | 494623.1 | 6925781.04 | 54.2 | 0 | 0 |
| 1756 | GF | 494626.69 | 6925770.22 | 54.3 | 0 | 0 |
| 1756 | FF | 494626.69 | 6925770.22 | 54.3 | 0 | 0 |
| 1757 | GF | 494630.07 | 6925760.98 | 54.4 | 0 | 0 |
| 1757 | FF | 494630.07 | 6925760.98 | 54.4 | 0 | 0 |
| 1758 | GF | 494637.47 | 6925749.24 | 54.6 | 0 | 0 |
| 1758 | FF | 494637.47 | 6925749.24 | 54.6 | 0 | 0 |
| 1759 | GF | 494633.5 | 6925728.26 | 55.2 | 0 | 0 |
| 1759 | FF | 494633.5 | 6925728.26 | 55.2 | 0 | 0 |
| 1760 | GF | 494624.91 | 6925715.96 | 56.1 | 0 | 0 |
| 1760 | FF | 494624.91 | 6925715.96 | 56.1 | 0 | 0 |
| 1761 | GF | 494617.83 | 6925706.03 | 56.7 | 0 | 0 |
| 1761 | FF | 494617.83 | 6925706.03 | 56.7 | 0 | 0 |
| 1762 | GF | 494605.85 | 6925694.7 | 57.8 | 0 | 0 |
| 1762 | FF | 494605.85 | 6925694.7 | 57.8 | 1 | 0 |
| 1763 | GF | 494585.4 | 6925679.65 | 60.3 | 1 | 0 |
| 1763 | FF | 494585.4 | 6925679.65 | 60.3 | 1 | 1 |
| 1764 | GF | 494568.32 | 6925633.19 | 60.5 | 2 | 1 |
| 1764 | FF | 494568.32 | 6925633.19 | 60.5 | 3 | 3 |
| 1765 | GF | 494558.12 | 6925640.36 | 60.5 | 2 | 1 |
| 1765 | FF | 494558.12 | 6925640.36 | 60.5 | 3 | 3 |
| 1766 | GF | 494549.62 | 6925646.27 | 60.7 | 2 | 1 |
| 1766 | FF | 494549.62 | 6925646.27 | 60.7 | 3 | 3 |
| 1767 | GF | 494540.6 | 6925652.85 | 60.9 | 3 | 1 |
| 1767 | FF | 494540.6 | 6925652.85 | 60.9 | 3 | 3 |
| 1768 | GF | 494530.39 | 6925660.93 | 61.0 | 3 | 1 |
| 1768 | FF | 494530.39 | 6925660.93 | 61.0 | 3 | 3 |
| 1769 | GF | 494519.23 | 6925669.69 | 61.1 | 3 | 1 |
| 1769 | FF | 494519.23 | 6925669.69 | 61.1 | 3 | 3 |
| 1770 | GF | 494508.34 | 6925677.4 | 61.5 | 3 | 1 |
| 1770 | FF | 494508.34 | 6925677.4 | 61.5 | 3 | 3 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1773 | GF | 494573.72 | 6925687.88 | 60.4 | 1 | 0 |
| 1773 | FF | 494573.72 | 6925687.88 | 60.4 | 1 | 1 |
| 1774 | GF | 494560.97 | 6925697.88 | 60.1 | 1 | 0 |
| 1774 | FF | 494560.97 | 6925697.88 | 60.1 | 1 | 1 |
| 1775 | GF | 494583.56 | 6925711.16 | 58.2 | 0 | 0 |
| 1775 | FF | 494583.56 | 6925711.16 | 58.2 | 1 | 0 |
| 1776 | GF | 494591.75 | 6925723.8 | 57.2 | 0 | 0 |
| 1776 | FF | 494591.75 | 6925723.8 | 57.2 | 0 | 0 |
| 1777 | GF | 494604.35 | 6925742.65 | 57.0 | 0 | 0 |
| 1777 | FF | 494604.35 | 6925742.65 | 57.0 | 0 | 0 |
| 1778 | GF | 494600.77 | 6925759.98 | 56.5 | 0 | 0 |
| 1778 | FF | 494600.77 | 6925759.98 | 56.5 | 0 | 0 |
| 1779 | GF | 494597.19 | 6925771.57 | 56.0 | 0 | 0 |
| 1779 | FF | 494597.19 | 6925771.57 | 56.0 | 0 | 0 |
| 1780 | GF | 494592.56 | 6925783.99 | 55.5 | 0 | 0 |
| 1780 | FF | 494592.56 | 6925783.99 | 55.5 | 0 | 0 |
| 1783 | GF | 494582.24 | 6925816.33 | 54.2 | 0 | 0 |
| 1783 | FF | 494582.24 | 6925816.33 | 54.2 | 0 | 0 |
| 1784 | GF | 494575.1 | 6925857.14 | 52.6 | 0 | 0 |
| 1784 | FF | 494575.1 | 6925857.14 | 52.6 | 1 | 0 |
| 1785 | GF | 494562.06 | 6925852.25 | 52.9 | 0 | 0 |
| 1785 | FF | 494562.06 | 6925852.25 | 52.9 | 1 | 0 |
| 1786 | GF | 494529.58 | 6925840.98 | 53.2 | 0 | 0 |
| 1786 | FF | 494529.58 | 6925840.98 | 53.2 | 1 | 0 |
| 1787 | GF | 494516.25 | 6925836.35 | 53.8 | 0 | 0 |
| 1787 | FF | 494516.25 | 6925836.35 | 53.8 | 1 | 0 |
| 1788 | GF | 494532.29 | 6925811.62 | 54.3 | 0 | 0 |
| 1788 | FF | 494532.29 | 6925811.62 | 54.3 | 0 | 0 |
| 1789 | GF | 494536.67 | 6925800.06 | 54.8 | 0 | 0 |
| 1789 | FF | 494536.67 | 6925800.06 | 54.8 | 0 | 0 |
| 1790 | GF | 494540.51 | 6925787.23 | 55.4 | 0 | 0 |
| 1790 | FF | 494540.51 | 6925787.23 | 55.4 | 0 | 0 |
| 1791 | GF | 494544.44 | 6925775.51 | 55.9 | 0 | 0 |
| 1791 | FF | 494544.44 | 6925775.51 | 55.9 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1792 | GF | 494554.69 | 6925759.53 | 56.4 | 0 | 0 |
| 1792 | FF | 494554.69 | 6925759.53 | 56.4 | 0 | 0 |
| 1793 | GF | 494546.89 | 6925742.4 | 58.0 | 0 | 0 |
| 1793 | FF | 494546.89 | 6925742.4 | 58.0 | 1 | 0 |
| 1794 | GF | 494523.13 | 6925757.28 | 58.0 | 0 | 0 |
| 1794 | FF | 494523.13 | 6925757.28 | 58.0 | 1 | 0 |
| 1795 | GF | 494516.45 | 6925767.18 | 57.5 | 0 | 0 |
| 1795 | FF | 494516.45 | 6925767.18 | 57.5 | 0 | 0 |
| 1796 | GF | 494512.77 | 6925777.94 | 56.6 | 0 | 0 |
| 1796 | FF | 494512.77 | 6925777.94 | 56.6 | 0 | 0 |
| 1797 | GF | 494509.06 | 6925789.91 | 56.1 | 0 | 0 |
| 1797 | FF | 494509.06 | 6925789.91 | 56.1 | 0 | 0 |
| 1798 | GF | 494505.53 | 6925801.2 | 55.6 | 0 | 0 |
| 1798 | FF | 494505.53 | 6925801.2 | 55.6 | 0 | 0 |
| 1799 | GF | 494502.86 | 6925832.14 | 54.1 | 0 | 0 |
| 1799 | FF | 494502.86 | 6925832.14 | 54.1 | 1 | 0 |
| 1800 | GF | 494490.23 | 6925827.19 | 54.3 | 0 | 0 |
| 1800 | FF | 494490.23 | 6925827.19 | 54.3 | 1 | 0 |
| 1801 | GF | 494456.75 | 6925815.99 | 55.1 | 1 | 0 |
| 1801 | FF | 494456.75 | 6925815.99 | 55.1 | 1 | 1 |
| 1802 | GF | 494444.38 | 6925810.58 | 55.6 | 1 | 0 |
| 1802 | FF | 494444.38 | 6925810.58 | 55.6 | 1 | 1 |
| 1803 | GF | 494430.83 | 6925806.96 | 56.7 | 1 | 1 |
| 1803 | FF | 494430.83 | 6925806.96 | 56.7 | 1 | 1 |
| 1804 | GF | 494420.32 | 6925802.97 | 57.3 | 1 | 1 |
| 1804 | FF | 494420.32 | 6925802.97 | 57.3 | 1 | 1 |
| 1805 | GF | 494409.44 | 6925799.27 | 57.8 | 1 | 1 |
| 1805 | FF | 494409.44 | 6925799.27 | 57.8 | 1 | 1 |
| 1806 | GF | 494456.77 | 6925793.96 | 55.7 | 0 | 0 |
| 1806 | FF | 494456.77 | 6925793.96 | 55.7 | 0 | 0 |
| 1807 | GF | 494460.96 | 6925783.04 | 56.2 | 0 | 0 |
| 1807 | FF | 494460.96 | 6925783.04 | 56.2 | 0 | 0 |
| 1808 | GF | 494464.45 | 6925771.64 | 56.5 | 0 | 0 |
| 1808 | FF | 494464.45 | 6925771.64 | 56.5 | 1 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1809 | GF | 494423.58 | 6925718.58 | 60.2 | 1 | 0 |
| 1809 | FF | 494423.58 | 6925718.58 | 60.2 | 3 | 2 |
| 1810 | GF | 494411.52 | 6925722.58 | 60.5 | 2 | 0 |
| 1810 | FF | 494411.52 | 6925722.58 | 60.5 | 3 | 3 |
| 1811 | GF | 494356.9 | 6925835.12 | 59.4 | 1 | 1 |
| 1811 | FF | 494356.9 | 6925835.12 | 59.4 | 1 | 1 |
| 1812 | GF | 494354.4 | 6925824.89 | 59.8 | 1 | 1 |
| 1812 | FF | 494354.4 | 6925824.89 | 59.8 | 1 | 1 |
| 1813 | GF | 494351.64 | 6925818.29 | 60.0 | 1 | 1 |
| 1813 | FF | 494351.64 | 6925818.29 | 60.0 | 1 | 1 |
| 1814 | GF | 494349.55 | 6925811.44 | 60.4 | 1 | 1 |
| 1814 | FF | 494349.55 | 6925811.44 | 60.4 | 1 | 1 |
| 1815 | GF | 494346.65 | 6925804.5 | 60.9 | 1 | 1 |
| 1815 | FF | 494346.65 | 6925804.5 | 60.9 | 1 | 1 |
| 1816 | GF | 494343.99 | 6925797.58 | 61.4 | 1 | 1 |
| 1816 | FF | 494343.99 | 6925797.58 | 61.4 | 2 | 1 |
| 1817 | GF | 494339.6 | 6925788.15 | 61.8 | 1 | 1 |
| 1817 | FF | 494339.6 | 6925788.15 | 61.8 | 2 | 1 |
| 1818 | GF | 494317.33 | 6925808.37 | 61.6 | 1 | 0 |
| 1818 | FF | 494317.33 | 6925808.37 | 61.6 | 1 | 1 |
| 1819 | GF | 494304.83 | 6925813.47 | 62.8 | 1 | 0 |
| 1819 | FF | 494304.83 | 6925813.47 | 62.8 | 1 | 1 |
| 1820 | GF | 494294.59 | 6925817.77 | 63.0 | 1 | 0 |
| 1820 | FF | 494294.59 | 6925817.77 | 63.0 | 1 | 1 |
| 1821 | GF | 494284.56 | 6925821.67 | 63.1 | 1 | 0 |
| 1821 | FF | 494284.56 | 6925821.67 | 63.1 | 1 | 1 |
| 1822 | GF | 494272.11 | 6925826.94 | 63.2 | 1 | 0 |
| 1822 | FF | 494272.11 | 6925826.94 | 63.2 | 1 | 1 |
| 1823 | GF | 494332.78 | 6925753.21 | 62.5 | 3 | 2 |
| 1823 | FF | 494332.78 | 6925753.21 | 62.5 | 3 | 3 |
| 1824 | GF | 494318.12 | 6925757.71 | 62.5 | 3 | 1 |
| 1824 | FF | 494318.12 | 6925757.71 | 62.5 | 3 | 3 |
| 1825 | GF | 494305.83 | 6925762.5 | 62.7 | 3 | 1 |
| 1825 | FF | 494305.83 | 6925762.5 | 62.7 | 3 | 3 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1826 | GF | 494294.89 | 6925766.61 | 62.8 | 3 | 1 |
| 1826 | FF | 494294.89 | 6925766.61 | 62.8 | 3 | 3 |
| 1827 | GF | 494284.64 | 6925770.88 | 63.0 | 3 | 1 |
| 1827 | FF | 494284.64 | 6925770.88 | 63.0 | 3 | 3 |
| 1828 | GF | 494273.53 | 6925775.07 | 63.1 | 3 | 1 |
| 1828 | FF | 494273.53 | 6925775.07 | 63.1 | 3 | 3 |
| 1829 | GF | 494262.26 | 6925778.88 | 63.2 | 3 | 1 |
| 1829 | FF | 494262.26 | 6925778.88 | 63.2 | 3 | 3 |
| 1830 | GF | 494252.89 | 6925783.07 | 63.4 | 3 | 1 |
| 1830 | FF | 494252.89 | 6925783.07 | 63.4 | 3 | 3 |
| 1831 | GF | 494241.6 | 6925787.23 | 63.5 | 3 | 1 |
| 1831 | FF | 494241.6 | 6925787.23 | 63.5 | 3 | 3 |
| 1832 | GF | 494231.29 | 6925791.04 | 63.6 | 3 | 1 |
| 1832 | FF | 494231.29 | 6925791.04 | 63.6 | 3 | 3 |
| 1833 | GF | 494221.59 | 6925795.31 | 63.8 | 3 | 1 |
| 1833 | FF | 494221.59 | 6925795.31 | 63.8 | 3 | 3 |
| 1834 | GF | 494210.98 | 6925798.92 | 63.9 | 3 | 1 |
| 1834 | FF | 494210.98 | 6925798.92 | 63.9 | 3 | 3 |
| 1835 | GF | 494154.1 | 6925816.59 | 64.8 | 3 | 2 |
| 1835 | FF | 494154.1 | 6925816.59 | 64.8 | 3 | 3 |
| 1836 | GF | 494151.28 | 6925830.95 | 64.5 | 2 | 1 |
| 1836 | FF | 494151.28 | 6925830.95 | 64.5 | 3 | 3 |
| 1837 | GF | 494149.78 | 6925842.6 | 64.5 | 2 | 1 |
| 1837 | FF | 494149.78 | 6925842.6 | 64.5 | 3 | 3 |
| 1838 | GF | 494149.67 | 6925854.32 | 64.6 | 2 | 1 |
| 1838 | FF | 494149.67 | 6925854.32 | 64.6 | 3 | 3 |
| 1839 | GF | 494147.09 | 6925867.6 | 64.7 | 2 | 1 |
| 1839 | FF | 494147.09 | 6925867.6 | 64.7 | 3 | 3 |
| 1840 | GF | 494144.88 | 6925879.21 | 64.6 | 2 | 1 |
| 1840 | FF | 494144.88 | 6925879.21 | 64.6 | 3 | 3 |
| 1841 | GF | 494142.03 | 6925890.34 | 64.5 | 2 | 1 |
| 1841 | FF | 494142.03 | 6925890.34 | 64.5 | 3 | 3 |
| 1842 | GF | 494138.86 | 6925905.2 | 64.4 | 2 | 0 |
| 1842 | FF | 494138.86 | 6925905.2 | 64.4 | 3 | 3 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1843 | GF | 494133.87 | 6925917.51 | 64.2 | 2 | 0 |
| 1843 | FF | 494133.87 | 6925917.51 | 64.2 | 3 | 3 |
| 1844 | GF | 494128.7 | 6925928.52 | 64.1 | 2 | 0 |
| 1844 | FF | 494128.7 | 6925928.52 | 64.1 | 3 | 3 |
| 1845 | GF | 494123.98 | 6925936.71 | 64.0 | 2 | 0 |
| 1845 | FF | 494123.98 | 6925936.71 | 64.0 | 3 | 3 |
| 1846 | GF | 494120.43 | 6925948.55 | 63.8 | 2 | 0 |
| 1846 | FF | 494120.43 | 6925948.55 | 63.8 | 3 | 3 |
| 1847 | GF | 494116.49 | 6925959.12 | 63.7 | 2 | 0 |
| 1847 | FF | 494116.49 | 6925959.12 | 63.7 | 3 | 3 |
| 1848 | GF | 494235.73 | 6925841.02 | 63.7 | 1 | 0 |
| 1848 | FF | 494235.73 | 6925841.02 | 63.7 | 1 | 1 |
| 1849 | GF | 494246.67 | 6925836.01 | 63.6 | 1 | 0 |
| 1849 | FF | 494246.67 | 6925836.01 | 63.6 | 1 | 1 |
| 1850 | GF | 494259.17 | 6925830.67 | 63.4 | 1 | 0 |
| 1850 | FF | 494259.17 | 6925830.67 | 63.4 | 1 | 1 |
| 1851 | GF | 494256.95 | 6925862.12 | 61.2 | 0 | 0 |
| 1851 | FF | 494256.95 | 6925862.12 | 61.2 | 1 | 0 |
| 1852 | GF | 494269.49 | 6925857.48 | 61.3 | 0 | 0 |
| 1852 | FF | 494269.49 | 6925857.48 | 61.3 | 1 | 0 |
| 1853 | GF | 494282.82 | 6925851.96 | 61.3 | 0 | 0 |
| 1853 | FF | 494282.82 | 6925851.96 | 61.3 | 1 | 0 |
| 1854 | GF | 494294.76 | 6925848.06 | 61.2 | 0 | 0 |
| 1854 | FF | 494294.76 | 6925848.06 | 61.2 | 1 | 0 |
| 1855 | GF | 494305.36 | 6925844.18 | 61.0 | 0 | 0 |
| 1855 | FF | 494305.36 | 6925844.18 | 61.0 | 1 | 0 |
| 1856 | GF | 494315.97 | 6925839.32 | 60.4 | 0 | 0 |
| 1856 | FF | 494315.97 | 6925839.32 | 60.4 | 1 | 0 |
| 1857 | GF | 494328.25 | 6925835.23 | 59.8 | 0 | 0 |
| 1857 | FF | 494328.25 | 6925835.23 | 59.8 | 1 | 0 |
| 1858 | GF | 494330.24 | 6925879.86 | 58.6 | 0 | 0 |
| 1858 | FF | 494330.24 | 6925879.86 | 58.6 | 0 | 0 |
| 1859 | GF | 494335.98 | 6925890.61 | 58.1 | 0 | 0 |
| 1859 | FF | 494335.98 | 6925890.61 | 58.1 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1860 | GF | 494338.62 | 6925900.94 | 57.5 | 0 | 0 |
| 1860 | FF | 494338.62 | 6925900.94 | 57.5 | 0 | 0 |
| 1861 | GF | 494342.65 | 6925911.44 | 56.9 | 0 | 0 |
| 1861 | FF | 494342.65 | 6925911.44 | 56.9 | 0 | 0 |
| 1862 | GF | 494347.89 | 6925923.56 | 56.1 | 0 | 0 |
| 1862 | FF | 494347.89 | 6925923.56 | 56.1 | 0 | 0 |
| 1863 | GF | 494352.14 | 6925936.17 | 55.4 | 0 | 0 |
| 1863 | FF | 494352.14 | 6925936.17 | 55.4 | 0 | 0 |
| 1864 | GF | 494356.7 | 6925946.65 | 54.8 | 0 | 0 |
| 1864 | FF | 494356.7 | 6925946.65 | 54.8 | 0 | 0 |
| 1865 | GF | 494360.2 | 6925957.17 | 54.0 | 0 | 0 |
| 1865 | FF | 494360.2 | 6925957.17 | 54.0 | 0 | 0 |
| 1866 | GF | 494364.68 | 6925968.83 | 53.4 | 0 | 0 |
| 1866 | FF | 494364.68 | 6925968.83 | 53.4 | 0 | 0 |
| 1867 | GF | 494377.5 | 6925982.69 | 51.8 | 0 | 0 |
| 1867 | FF | 494377.5 | 6925982.69 | 51.8 | 0 | 0 |
| 1868 | GF | 494365.01 | 6925987.23 | 51.6 | 0 | 0 |
| 1868 | FF | 494365.01 | 6925987.23 | 51.6 | 0 | 0 |
| 1869 | GF | 494351.74 | 6925992.54 | 51.3 | 0 | 0 |
| 1869 | FF | 494351.74 | 6925992.54 | 51.3 | 0 | 0 |
| 1870 | GF | 494337.55 | 6925997.95 | 50.5 | 0 | 0 |
| 1870 | FF | 494337.55 | 6925997.95 | 50.5 | 0 | 0 |
| 1871 | GF | 494338.14 | 6925979.31 | 51.4 | 0 | 0 |
| 1871 | FF | 494338.14 | 6925979.31 | 51.4 | 0 | 0 |
| 1872 | GF | 494333.66 | 6925967.64 | 52.2 | 0 | 0 |
| 1872 | FF | 494333.66 | 6925967.64 | 52.2 | 0 | 0 |
| 1873 | GF | 494329.63 | 6925957.14 | 53.1 | 0 | 0 |
| 1873 | FF | 494329.63 | 6925957.14 | 53.1 | 0 | 0 |
| 1874 | GF | 494325.6 | 6925946.65 | 53.7 | 0 | 0 |
| 1874 | FF | 494325.6 | 6925946.65 | 53.7 | 0 | 0 |
| 1875 | GF | 494320.74 | 6925934 | 54.6 | 0 | 0 |
| 1875 | FF | 494320.74 | 6925934 | 54.6 | 0 | 0 |
| 1876 | GF | 494316.95 | 6925921.45 | 56.0 | 0 | 0 |
| 1876 | FF | 494316.95 | 6925921.45 | 56.0 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1877 | GF | 494311.71 | 6925910.84 | 56.7 | 0 | 0 |
| 1877 | FF | 494311.71 | 6925910.84 | 56.7 | 0 | 0 |
| 1878 | GF | 494308.48 | 6925901.05 | 58.0 | 0 | 0 |
| 1878 | FF | 494308.48 | 6925901.05 | 58.0 | 0 | 0 |
| 1879 | GF | 494302.83 | 6925890.51 | 59.4 | 0 | 0 |
| 1879 | FF | 494302.83 | 6925890.51 | 59.4 | 0 | 0 |
| 1881 | GF | 494215.74 | 6925916.74 | 61.5 | 0 | 0 |
| 1881 | FF | 494215.74 | 6925916.74 | 61.5 | 1 | 0 |
| 1882 | GF | 494225.09 | 6925923.11 | 60.7 | 0 | 0 |
| 1882 | FF | 494225.09 | 6925923.11 | 60.7 | 1 | 0 |
| 1883 | GF | 494233.78 | 6925929.01 | 59.8 | 0 | 0 |
| 1883 | FF | 494233.78 | 6925929.01 | 59.8 | 1 | 0 |
| 1884 | GF | 494242.85 | 6925935.17 | 58.6 | 0 | 0 |
| 1884 | FF | 494242.85 | 6925935.17 | 58.6 | 0 | 0 |
| 1885 | GF | 494253.77 | 6925942.64 | 57.2 | 0 | 0 |
| 1885 | FF | 494253.77 | 6925942.64 | 57.2 | 0 | 0 |
| 1886 | GF | 494264.77 | 6925950.25 | 55.8 | 0 | 0 |
| 1886 | FF | 494264.77 | 6925950.25 | 55.8 | 0 | 0 |
| 1887 | GF | 494274.2 | 6925956.01 | 55.0 | 0 | 0 |
| 1887 | FF | 494274.2 | 6925956.01 | 55.0 | 0 | 0 |
| 1888 | GF | 494285.6 | 6925970.91 | 53.2 | 0 | 0 |
| 1888 | FF | 494285.6 | 6925970.91 | 53.2 | 0 | 0 |
| 1889 | GF | 494292.28 | 6925986.27 | 52.1 | 0 | 0 |
| 1889 | FF | 494292.28 | 6925986.27 | 52.1 | 0 | 0 |
| 1890 | GF | 494295.47 | 6925996.4 | 51.1 | 0 | 0 |
| 1890 | FF | 494295.47 | 6925996.4 | 51.1 | 0 | 0 |
| 1891 | GF | 494304.39 | 6926011.05 | 50.2 | 0 | 0 |
| 1891 | FF | 494304.39 | 6926011.05 | 50.2 | 0 | 0 |
| 1892 | GF | 494290.07 | 6926016.76 | 50.1 | 0 | 0 |
| 1892 | FF | 494290.07 | 6926016.76 | 50.1 | 0 | 0 |
| 1893 | GF | 494276.44 | 6926021.39 | 50.2 | 0 | 0 |
| 1893 | FF | 494276.44 | 6926021.39 | 50.2 | 0 | 0 |
| 1894 | GF | 494272.77 | 6926004.26 | 51.1 | 0 | 0 |
| 1894 | FF | 494272.77 | 6926004.26 | 51.1 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1895 | GF | 494268.27 | 6925991 | 51.8 | 0 | 0 |
| 1895 | FF | 494268.27 | 6925991 | 51.8 | 0 | 0 |
| 1896 | GF | 494257.69 | 6925979.47 | 52.7 | 0 | 0 |
| 1896 | FF | 494257.69 | 6925979.47 | 52.7 | 0 | 0 |
| 1897 | GF | 494248.83 | 6925972.78 | 53.4 | 0 | 0 |
| 1897 | FF | 494248.83 | 6925972.78 | 53.4 | 0 | 0 |
| 1898 | GF | 494237.09 | 6925965.29 | 54.8 | 0 | 0 |
| 1898 | FF | 494237.09 | 6925965.29 | 54.8 | 0 | 0 |
| 1899 | GF | 494226.93 | 6925957.84 | 56.5 | 0 | 0 |
| 1899 | FF | 494226.93 | 6925957.84 | 56.5 | 0 | 0 |
| 1900 | GF | 494217.63 | 6925951.31 | 58.1 | 0 | 0 |
| 1900 | FF | 494217.63 | 6925951.31 | 58.1 | 0 | 0 |
| 1901 | GF | 494208.38 | 6925945.72 | 59.4 | 0 | 0 |
| 1901 | FF | 494208.38 | 6925945.72 | 59.4 | 1 | 0 |
| 1903 | GF | 494191.76 | 6925992.51 | 57.4 | 0 | 0 |
| 1903 | FF | 494191.76 | 6925992.51 | 57.4 | 0 | 0 |
| 1904 | GF | 494201.28 | 6925998.87 | 56.1 | 0 | 0 |
| 1904 | FF | 494201.28 | 6925998.87 | 56.1 | 0 | 0 |
| 1905 | GF | 494218.61 | 6926010.98 | 53.3 | 0 | 0 |
| 1905 | FF | 494218.61 | 6926010.98 | 53.3 | 0 | 0 |
| 1906 | GF | 494232.99 | 6926020.9 | 51.6 | 0 | 0 |
| 1906 | FF | 494232.99 | 6926020.9 | 51.6 | 0 | 0 |
| 1907 | GF | 494239.96 | 6926039.21 | 50.6 | 0 | 0 |
| 1907 | FF | 494239.96 | 6926039.21 | 50.6 | 0 | 0 |
| 1908 | GF | 494226.24 | 6926060.68 | 51.0 | 0 | 0 |
| 1908 | FF | 494226.24 | 6926060.68 | 51.0 | 0 | 0 |
| 1909 | GF | 494214.15 | 6926040.83 | 53.4 | 0 | 0 |
| 1909 | FF | 494214.15 | 6926040.83 | 53.4 | 0 | 0 |
| 1910 | GF | 494203.19 | 6926034.7 | 55.0 | 0 | 0 |
| 1910 | FF | 494203.19 | 6926034.7 | 55.0 | 0 | 0 |
| 1911 | GF | 494193.1 | 6926027.84 | 56.5 | 0 | 0 |
| 1911 | FF | 494193.1 | 6926027.84 | 56.5 | 0 | 0 |
| 1912 | GF | 494185.15 | 6926021.6 | 57.7 | 0 | 0 |
| 1912 | FF | 494185.15 | 6926021.6 | 57.7 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1913 | GF | 494176.04 | 6926015.11 | 59.0 | 0 | 0 |
| 1913 | FF | 494176.04 | 6926015.11 | 59.0 | 1 | 0 |
| 1914 | GF | 494110.51 | 6925969.04 | 63.6 | 2 | 0 |
| 1914 | FF | 494110.51 | 6925969.04 | 63.6 | 3 | 3 |
| 1915 | GF | 494105.67 | 6925979.11 | 63.5 | 2 | 0 |
| 1915 | FF | 494105.67 | 6925979.11 | 63.5 | 3 | 3 |
| 1916 | GF | 494094.1 | 6926010.47 | 63.1 | 2 | 1 |
| 1916 | FF | 494094.1 | 6926010.47 | 63.1 | 3 | 3 |
| 1917 | GF | 494086.9 | 6926023.93 | 62.9 | 2 | 1 |
| 1917 | FF | 494086.9 | 6926023.93 | 62.9 | 3 | 3 |
| 1918 | GF | 494080.7 | 6926035.91 | 62.7 | 2 | 1 |
| 1918 | FF | 494080.7 | 6926035.91 | 62.7 | 3 | 3 |
| 1919 | GF | 494075 | 6926047.51 | 62.6 | 2 | 1 |
| 1919 | FF | 494075 | 6926047.51 | 62.6 | 3 | 3 |
| 1920 | GF | 494067.44 | 6926058.58 | 62.4 | 2 | 1 |
| 1920 | FF | 494067.44 | 6926058.58 | 62.4 | 3 | 3 |
| 1921 | GF | 494058.36 | 6926068.32 | 62.3 | 2 | 1 |
| 1921 | FF | 494058.36 | 6926068.32 | 62.3 | 3 | 3 |
| 1922 | GF | 494049.64 | 6926080.27 | 62.1 | 2 | 1 |
| 1922 | FF | 494049.64 | 6926080.27 | 62.1 | 3 | 3 |
| 1923 | GF | 494042.87 | 6926089.75 | 61.9 | 2 | 1 |
| 1923 | FF | 494042.87 | 6926089.75 | 61.9 | 3 | 3 |
| 1924 | GF | 494035.23 | 6926099.04 | 61.6 | 2 | 1 |
| 1924 | FF | 494035.23 | 6926099.04 | 61.6 | 3 | 3 |
| 1925 | GF | 494027.46 | 6926107.93 | 61.3 | 2 | 0 |
| 1925 | FF | 494027.46 | 6926107.93 | 61.3 | 3 | 3 |
| 1926 | GF | 494019.86 | 6926116.21 | 61.3 | 2 | 0 |
| 1926 | FF | 494019.86 | 6926116.21 | 61.3 | 3 | 3 |
| 1927 | GF | 494011.41 | 6926124.4 | 60.7 | 2 | 0 |
| 1927 | FF | 494011.41 | 6926124.4 | 60.7 | 3 | 3 |
| 1928 | GF | 493994.35 | 6926140.78 | 61.9 | 1 | 0 |
| 1928 | FF | 493994.35 | 6926140.78 | 61.9 | 3 | 2 |
| 1929 | GF | 494154.58 | 6926060.7 | 58.1 | 0 | 0 |
| 1929 | FF | 494154.58 | 6926060.7 | 58.1 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1930 | GF | 494165.65 | 6926067.94 | 56.5 | 0 | 0 |
| 1930 | FF | 494165.65 | 6926067.94 | 56.5 | 0 | 0 |
| 1931 | GF | 494176.37 | 6926076.88 | 54.9 | 0 | 0 |
| 1931 | FF | 494176.37 | 6926076.88 | 54.9 | 0 | 0 |
| 1933 | GF | 494186.38 | 6926082.77 | 53.4 | 0 | 0 |
| 1933 | FF | 494186.38 | 6926082.77 | 53.4 | 0 | 0 |
| 1934 | GF | 494196.87 | 6926090.44 | 52.1 | 0 | 0 |
| 1934 | FF | 494196.87 | 6926090.44 | 52.1 | 0 | 0 |
| 1935 | GF | 494214.17 | 6926094.71 | 51.0 | 0 | 0 |
| 1935 | FF | 494214.17 | 6926094.71 | 51.0 | 0 | 0 |
| 1936 | GF | 494208.66 | 6926108.48 | 51.2 | 0 | 0 |
| 1936 | FF | 494208.66 | 6926108.48 | 51.2 | 0 | 0 |
| 1937 | GF | 494196.54 | 6926135.15 | 51.8 | 0 | 0 |
| 1937 | FF | 494196.54 | 6926135.15 | 51.8 | 0 | 0 |
| 1938 | GF | 494183.76 | 6926113.94 | 53.2 | 0 | 0 |
| 1938 | FF | 494183.76 | 6926113.94 | 53.2 | 0 | 0 |
| 1939 | GF | 494170.3 | 6926105.05 | 54.9 | 0 | 0 |
| 1939 | FF | 494170.3 | 6926105.05 | 54.9 | 0 | 0 |
| 1940 | GF | 494159.44 | 6926096.88 | 56.4 | 0 | 0 |
| 1940 | FF | 494159.44 | 6926096.88 | 56.4 | 0 | 0 |
| 1941 | GF | 494148.44 | 6926089.6 | 57.5 | 0 | 0 |
| 1941 | FF | 494148.44 | 6926089.6 | 57.5 | 0 | 0 |
| 1942 | GF | 494138.01 | 6926082.78 | 58.8 | 0 | 0 |
| 1942 | FF | 494138.01 | 6926082.78 | 58.8 | 1 | 0 |
| 1943 | GF | 494127.33 | 6926075.17 | 60.0 | 0 | 0 |
| 1943 | FF | 494127.33 | 6926075.17 | 60.0 | 1 | 0 |
| 1944 | GF | 494122.73 | 6926130.53 | 57.7 | 0 | 0 |
| 1944 | FF | 494122.73 | 6926130.53 | 57.7 | 0 | 0 |
| 1945 | GF | 494131.14 | 6926136.19 | 56.7 | 0 | 0 |
| 1945 | FF | 494131.14 | 6926136.19 | 56.7 | 0 | 0 |
| 1946 | GF | 494139.48 | 6926142.75 | 55.8 | 0 | 0 |
| 1946 | FF | 494139.48 | 6926142.75 | 55.8 | 0 | 0 |
| 1947 | GF | 494149.17 | 6926148.94 | 54.5 | 0 | 0 |
| 1947 | FF | 494149.17 | 6926148.94 | 54.5 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1948 | GF | 494158.23 | 6926155.06 | 53.5 | 0 | 0 |
| 1948 | FF | 494158.23 | 6926155.06 | 53.5 | 0 | 0 |
| 1949 | GF | 494166.48 | 6926160.76 | 52.6 | 0 | 0 |
| 1949 | FF | 494166.48 | 6926160.76 | 52.6 | 0 | 0 |
| 1950 | GF | 494185.34 | 6926163.62 | 51.9 | 0 | 0 |
| 1950 | FF | 494185.34 | 6926163.62 | 51.9 | 0 | 0 |
| 1951 | GF | 494175.9 | 6926178.9 | 52.4 | 0 | 0 |
| 1951 | FF | 494175.9 | 6926178.9 | 52.4 | 0 | 0 |
| 1952 | GF | 494158.2 | 6926189.75 | 53.0 | 0 | 0 |
| 1952 | FF | 494158.2 | 6926189.75 | 53.0 | 0 | 0 |
| 1953 | GF | 494141.88 | 6926193.94 | 53.3 | 0 | 0 |
| 1953 | FF | 494141.88 | 6926193.94 | 53.3 | 0 | 0 |
| 1954 | GF | 494126.75 | 6926195.3 | 53.8 | 0 | 0 |
| 1954 | FF | 494126.75 | 6926195.3 | 53.8 | 0 | 0 |
| 1955 | GF | 494114.11 | 6926160.18 | 55.9 | 0 | 0 |
| 1955 | FF | 494114.11 | 6926160.18 | 55.9 | 0 | 0 |
| 1956 | GF | 494105.36 | 6926153.68 | 56.5 | 0 | 0 |
| 1956 | FF | 494105.36 | 6926153.68 | 56.5 | 0 | 0 |
| 1957 | GF | 494095.16 | 6926147 | 57.6 | 0 | 0 |
| 1957 | FF | 494095.16 | 6926147 | 57.6 | 0 | 0 |
| 1958 | GF | 494086.7 | 6926141.28 | 58.3 | 0 | 0 |
| 1958 | FF | 494086.7 | 6926141.28 | 58.3 | 1 | 0 |
| 1959 | GF | 494078.46 | 6926135.53 | 59.0 | 0 | 0 |
| 1959 | FF | 494078.46 | 6926135.53 | 59.0 | 1 | 0 |
| 1960 | GF | 494069.9 | 6926129.4 | 59.8 | 1 | 0 |
| 1960 | FF | 494069.9 | 6926129.4 | 59.8 | 1 | 0 |
| 1961 | GF | 494061.27 | 6926201.06 | 56.2 | 0 | 0 |
| 1961 | FF | 494061.27 | 6926201.06 | 56.2 | 0 | 0 |
| 1962 | GF | 494050.24 | 6926202.38 | 56.8 | 0 | 0 |
| 1962 | FF | 494050.24 | 6926202.38 | 56.8 | 0 | 0 |
| 1963 | GF | 494040.53 | 6926203.23 | 57.5 | 0 | 0 |
| 1963 | FF | 494040.53 | 6926203.23 | 57.5 | 1 | 0 |
| 1964 | GF | 494028.57 | 6926204.98 | 58.1 | 0 | 0 |
| 1964 | FF | 494028.57 | 6926204.98 | 58.1 | 1 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1965 | GF | 494018.32 | 6926207.11 | 58.5 | 0 | 0 |
| 1965 | FF | 494018.32 | 6926207.11 | 58.5 | 1 | 0 |
| 1966 | GF | 494002.29 | 6926209.95 | 59.5 | 1 | 0 |
| 1966 | FF | 494002.29 | 6926209.95 | 59.5 | 1 | 1 |
| 1967 | GF | 494035.97 | 6926175.81 | 60.0 | 1 | 0 |
| 1967 | FF | 494035.97 | 6926175.81 | 60.0 | 1 | 0 |
| 1968 | GF | 494025.67 | 6926177.66 | 60.3 | 1 | 0 |
| 1968 | FF | 494025.67 | 6926177.66 | 60.3 | 1 | 1 |
| 1969 | GF | 494014.3 | 6926179.18 | 60.5 | 1 | 0 |
| 1969 | FF | 494014.3 | 6926179.18 | 60.5 | 1 | 1 |
| 1970 | GF | 493999.64 | 6926183.4 | 60.6 | 1 | 0 |
| 1970 | FF | 493999.64 | 6926183.4 | 60.6 | 1 | 1 |
| 1971 | GF | 493961.37 | 6926185.14 | 61.0 | 2 | 2 |
| 1971 | FF | 493961.37 | 6926185.14 | 61.0 | 3 | 2 |
| 1972 | GF | 493964.13 | 6926196.66 | 60.7 | 1 | 1 |
| 1972 | FF | 493964.13 | 6926196.66 | 60.7 | 2 | 2 |
| 1973 | GF | 493965.67 | 6926209.08 | 60.3 | 1 | 1 |
| 1973 | FF | 493965.67 | 6926209.08 | 60.3 | 2 | 1 |
| 1974 | GF | 493968.15 | 6926223.74 | 59.8 | 1 | 1 |
| 1974 | FF | 493968.15 | 6926223.74 | 59.8 | 1 | 1 |
| 1975 | GF | 493948.94 | 6926214.66 | 60.4 | 1 | 1 |
| 1975 | FF | 493948.94 | 6926214.66 | 60.4 | 2 | 2 |
| 1976 | GF | 493939.55 | 6926216.1 | 60.4 | 2 | 2 |
| 1976 | FF | 493939.55 | 6926216.1 | 60.4 | 2 | 2 |
| 1977 | GF | 495032.91 | 6925965.42 | 52.8 | 0 | 0 |
| 1977 | FF | 495032.91 | 6925965.42 | 52.8 | 0 | 0 |
| 1978 | GF | 495034.05 | 6925953.03 | 52.9 | 0 | 0 |
| 1978 | FF | 495034.05 | 6925953.03 | 52.9 | 1 | 0 |
| 1978 | GF | 494435.59 | 6925762.28 | 58.6 | 1 | 0 |
| 1978 | FF | 494435.59 | 6925762.28 | 58.6 | 1 | 0 |
| 1979 | GF | 494432.5 | 6925773.45 | 58.6 | 1 | 0 |
| 1979 | FF | 494432.5 | 6925773.45 | 58.6 | 1 | 0 |
| 1979 | GF | 495035.9 | 6925934.97 | 53.0 | 0 | 0 |
| 1979 | FF | 495035.9 | 6925934.97 | 53.0 | 1 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1980 | GF | 495080.88 | 6925964.86 | 52.4 | 0 | 0 |
| 1980 | FF | 495080.88 | 6925964.86 | 52.4 | 1 | 0 |
| 1980 | GF | 494427.69 | 6925784.22 | 58.7 | 1 | 0 |
| 1980 | FF | 494427.69 | 6925784.22 | 58.7 | 1 | 0 |
| 1981 | GF | 494370.31 | 6925738.6 | 61.2 | 2 | 1 |
| 1981 | FF | 494370.31 | 6925738.6 | 61.2 | 3 | 3 |
| 1981 | GF | 495091.34 | 6925977.25 | 51.9 | 0 | 0 |
| 1981 | FF | 495091.34 | 6925977.25 | 51.9 | 1 | 0 |
| 1982 | GF | 494381.12 | 6925734.65 | 60.9 | 2 | 1 |
| 1982 | FF | 494381.12 | 6925734.65 | 60.9 | 3 | 3 |
| 1982 | GF | 495099.25 | 6925991.92 | 51.5 | 0 | 0 |
| 1982 | FF | 495099.25 | 6925991.92 | 51.5 | 1 | 0 |
| 1983 | GF | 495105.84 | 6926005.34 | 51.1 | 0 | 0 |
| 1983 | FF | 495105.84 | 6926005.34 | 51.1 | 1 | 0 |
| 1983 | GF | 494391.36 | 6925730.04 | 60.9 | 2 | 0 |
| 1983 | FF | 494391.36 | 6925730.04 | 60.9 | 3 | 3 |
| 1984 | GF | 495106.07 | 6926020.72 | 50.5 | 0 | 0 |
| 1984 | FF | 495106.07 | 6926020.72 | 50.5 | 1 | 0 |
| 1984 | GF | 494401.31 | 6925726.47 | 60.8 | 2 | 0 |
| 1984 | FF | 494401.31 | 6925726.47 | 60.8 | 3 | 3 |
| 1985 | GF | 494101.27 | 6925989.11 | 63.4 | 2 | 1 |
| 1985 | FF | 494101.27 | 6925989.11 | 63.4 | 3 | 3 |
| 1985 | GF | 495096.85 | 6926040.48 | 49.1 | 0 | 0 |
| 1985 | FF | 495096.85 | 6926040.48 | 49.1 | 0 | 0 |
| 1986 | GF | 495083.25 | 6926039.36 | 49.2 | 0 | 0 |
| 1986 | FF | 495083.25 | 6926039.36 | 49.2 | 0 | 0 |
| 1986 | GF | 494203.03 | 6926121.87 | 51.6 | 0 | 0 |
| 1986 | FF | 494203.03 | 6926121.87 | 51.6 | 0 | 0 |
| 1987 | GF | 494209.63 | 6926004.83 | 54.8 | 0 | 0 |
| 1987 | FF | 494209.63 | 6926004.83 | 54.8 | 0 | 0 |
| 1987 | GF | 495069.65 | 6926038.37 | 48.9 | 0 | 0 |
| 1987 | FF | 495069.65 | 6926038.37 | 48.9 | 0 | 0 |
| 1988 | GF | 495074.98 | 6926018.86 | 50.4 | 0 | 0 |
| 1988 | FF | 495074.98 | 6926018.86 | 50.4 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 1989 | GF | 495074.86 | 6926008.23 | 51.2 | 0 | 0 |
| 1989 | FF | 495074.86 | 6926008.23 | 51.2 | 0 | 0 |
| 2522 | GF | 494724.21 | 6925879.91 | 50.0 | 0 | 0 |
| 2522 | FF | 494724.21 | 6925879.91 | 50.0 | 0 | 0 |
| 2523 | GF | 494736.23 | 6925884.39 | 49.4 | 0 | 0 |
| 2523 | FF | 494736.23 | 6925884.39 | 49.4 | 0 | 0 |
| 2524 | GF | 494699.13 | 6925798.39 | 51.9 | 0 | 0 |
| 2524 | FF | 494699.13 | 6925798.39 | 51.9 | 0 | 0 |
| 2525 | GF | 494744.28 | 6925814.21 | 51.5 | 0 | 0 |
| 2525 | FF | 494744.28 | 6925814.21 | 51.5 | 1 | 0 |
| 2526 | GF | 494771.51 | 6925821.07 | 49.6 | 1 | 1 |
| 2526 | FF | 494771.51 | 6925821.07 | 49.6 | 1 | 1 |
| 2527 | GF | 494743.01 | 6925547.6 | 58.4 | 3 | 2 |
| 2527 | FF | 494743.01 | 6925547.6 | 58.4 | 3 | 3 |
| 2528 | GF | 494709.75 | 6925577.27 | 58.4 | 1 | 1 |
| 2528 | FF | 494709.75 | 6925577.27 | 58.4 | 2 | 1 |
| 2529 | GF | 494686.19 | 6925595.24 | 58.3 | 1 | 0 |
| 2529 | FF | 494686.19 | 6925595.24 | 58.3 | 1 | 1 |
| 2530 | GF | 494648.42 | 6925622.53 | 58.9 | 1 | 0 |
| 2530 | FF | 494648.42 | 6925622.53 | 58.9 | 1 | 1 |
| 2531 | GF | 494656 | 6925632.47 | 57.9 | 0 | 0 |
| 2531 | FF | 494656 | 6925632.47 | 57.9 | 1 | 1 |
| 2532 | GF | 494622.75 | 6925639.42 | 59.4 | 1 | 0 |
| 2532 | FF | 494622.75 | 6925639.42 | 59.4 | 1 | 1 |
| 2533 | GF | 494630.33 | 6925649.35 | 58.6 | 0 | 0 |
| 2533 | FF | 494630.33 | 6925649.35 | 58.6 | 1 | 1 |
| 2534 | GF | 494595.67 | 6925671.78 | 59.2 | 1 | 0 |
| 2534 | FF | 494595.67 | 6925671.78 | 59.2 | 1 | 1 |
| 2535 | GF | 494670.59 | 6925788.95 | 53.8 | 0 | 0 |
| 2535 | FF | 494670.59 | 6925788.95 | 53.8 | 0 | 0 |
| 2536 | GF | 494659.09 | 6925808.89 | 53.0 | 0 | 0 |
| 2536 | FF | 494659.09 | 6925808.89 | 53.0 | 0 | 0 |
| 2537 | GF | 494714.86 | 6925905.28 | 50.2 | 0 | 0 |
| 2537 | FF | 494714.86 | 6925905.28 | 50.2 | 1 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 2538 | GF | 494727.29 | 6925908.91 | 49.7 | 0 | 0 |
| 2538 | FF | 494727.29 | 6925908.91 | 49.7 | 1 | 1 |
| 2539 | GF | 494575.95 | 6925831.17 | 53.5 | 0 | 0 |
| 2539 | FF | 494575.95 | 6925831.17 | 53.5 | 0 | 0 |
| 2540 | GF | 494529.25 | 6925819.77 | 53.8 | 0 | 0 |
| 2540 | FF | 494529.25 | 6925819.77 | 53.8 | 0 | 0 |
| 2541 | GF | 494585.97 | 6925804.82 | 54.7 | 0 | 0 |
| 2541 | FF | 494585.97 | 6925804.82 | 54.7 | 0 | 0 |
| 2542 | GF | 494502.24 | 6925810.75 | 55.1 | 0 | 0 |
| 2542 | FF | 494502.24 | 6925810.75 | 55.1 | 0 | 0 |
| 2543 | GF | 494881.13 | 6925948.72 | 49.6 | 0 | 0 |
| 2543 | FF | 494881.13 | 6925948.72 | 49.6 | 0 | 0 |
| 2544 | GF | 494881.33 | 6925935.01 | 49.5 | 0 | 0 |
| 2544 | FF | 494881.33 | 6925935.01 | 49.5 | 0 | 0 |
| 2545 | GF | 494883.83 | 6925891.37 | 49.2 | 0 | 0 |
| 2545 | FF | 494883.83 | 6925891.37 | 49.2 | 0 | 0 |
| 2546 | GF | 494932.53 | 6925864.55 | 49.8 | 0 | 0 |
| 2546 | FF | 494932.53 | 6925864.55 | 49.8 | 1 | 0 |
| 2547 | GF | 494953.51 | 6925886.31 | 51.5 | 0 | 0 |
| 2547 | FF | 494953.51 | 6925886.31 | 51.5 | 1 | 0 |
| 2548 | GF | 494962.34 | 6925865.65 | 51.6 | 0 | 0 |
| 2548 | FF | 494962.34 | 6925865.65 | 51.6 | 1 | 1 |
| 2549 | GF | 495029.86 | 6925888.79 | 53.3 | 1 | 0 |
| 2549 | FF | 495029.86 | 6925888.79 | 53.3 | 1 | 1 |
| 2550 | GF | 495041.61 | 6925891.73 | 53.3 | 1 | 0 |
| 2550 | FF | 495041.61 | 6925891.73 | 53.3 | 1 | 1 |
| 2551 | GF | 495052.59 | 6925894.44 | 53.3 | 1 | 0 |
| 2551 | FF | 495052.59 | 6925894.44 | 53.3 | 1 | 1 |
| 2552 | GF | 495064.57 | 6925898.31 | 53.2 | 1 | 0 |
| 2552 | FF | 495064.57 | 6925898.31 | 53.2 | 1 | 1 |
| 2553 | GF | 495114.24 | 6926039.68 | 49.0 | 0 | 0 |
| 2553 | FF | 495114.24 | 6926039.68 | 49.0 | 1 | 0 |
| 2554 | GF | 494200.28 | 6925803.05 | 64.1 | 3 | 1 |
| 2554 | FF | 494200.28 | 6925803.05 | 64.1 | 3 | 3 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 2555 | GF | 494189.16 | 6925806.7 | 64.2 | 3 | 1 |
| 2555 | FF | 494189.16 | 6925806.7 | 64.2 | 3 | 3 |
| 2556 | GF | 494221.72 | 6925844.44 | 63.8 | 1 | 0 |
| 2556 | FF | 494221.72 | 6925844.44 | 63.8 | 2 | 1 |
| 2557 | GF | 494204.01 | 6925852.69 | 63.6 | 1 | 0 |
| 2557 | FF | 494204.01 | 6925852.69 | 63.6 | 2 | 1 |
| 2558 | GF | 494245.42 | 6925865.89 | 61.1 | 0 | 0 |
| 2558 | FF | 494245.42 | 6925865.89 | 61.1 | 1 | 0 |
| 2559 | GF | 494227.59 | 6925872.78 | 61.4 | 1 | 0 |
| 2559 | FF | 494227.59 | 6925872.78 | 61.4 | 1 | 0 |
| 2560 | GF | 494186.46 | 6925894.13 | 63.6 | 1 | 0 |
| 2560 | FF | 494186.46 | 6925894.13 | 63.6 | 1 | 1 |
| 2561 | GF | 494195.43 | 6925903.76 | 63.1 | 1 | 0 |
| 2561 | FF | 494195.43 | 6925903.76 | 63.1 | 1 | 0 |
| 2562 | GF | 494205.4 | 6925910.12 | 62.4 | 1 | 0 |
| 2562 | FF | 494205.4 | 6925910.12 | 62.4 | 1 | 0 |
| 2563 | GF | 494179.75 | 6925926.34 | 63.2 | 1 | 0 |
| 2563 | FF | 494179.75 | 6925926.34 | 63.2 | 1 | 0 |
| 2564 | GF | 494190.41 | 6925931.1 | 62.1 | 1 | 0 |
| 2564 | FF | 494190.41 | 6925931.1 | 62.1 | 1 | 0 |
| 2565 | GF | 494199.6 | 6925938.64 | 60.5 | 0 | 0 |
| 2565 | FF | 494199.6 | 6925938.64 | 60.5 | 1 | 0 |
| 2566 | GF | 494160.85 | 6925970.9 | 62.0 | 1 | 0 |
| 2566 | FF | 494160.85 | 6925970.9 | 62.0 | 1 | 0 |
| 2567 | GF | 494171.14 | 6925977.56 | 60.6 | 0 | 0 |
| 2567 | FF | 494171.14 | 6925977.56 | 60.6 | 1 | 0 |
| 2568 | GF | 494181.64 | 6925985.53 | 59.0 | 0 | 0 |
| 2568 | FF | 494181.64 | 6925985.53 | 59.0 | 1 | 0 |
| 2569 | GF | 494233.21 | 6926050.34 | 50.8 | 0 | 0 |
| 2569 | FF | 494233.21 | 6926050.34 | 50.8 | 0 | 0 |
| 2570 | GF | 494166.35 | 6926008.93 | 60.4 | 0 | 0 |
| 2570 | FF | 494166.35 | 6926008.93 | 60.4 | 1 | 0 |
| 2571 | GF | 494157.82 | 6926003.56 | 61.5 | 1 | 0 |
| 2571 | FF | 494157.82 | 6926003.56 | 61.5 | 1 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 2572 | GF | 494148.35 | 6925998.36 | 62.3 | 1 | 0 |
| 2572 | FF | 494148.35 | 6925998.36 | 62.3 | 1 | 0 |
| 2573 | GF | 494130 | 6926042.99 | 61.3 | 1 | 0 |
| 2573 | FF | 494130 | 6926042.99 | 61.3 | 1 | 0 |
| 2574 | GF | 494143.17 | 6926052.77 | 59.7 | 0 | 0 |
| 2574 | FF | 494143.17 | 6926052.77 | 59.7 | 1 | 0 |
| 2575 | GF | 494114.04 | 6926067.68 | 61.2 | 1 | 0 |
| 2575 | FF | 494114.04 | 6926067.68 | 61.2 | 1 | 0 |
| 2576 | GF | 494084.56 | 6926106.38 | 61.4 | 1 | 0 |
| 2576 | FF | 494084.56 | 6926106.38 | 61.4 | 1 | 0 |
| 2577 | GF | 494095.73 | 6926112.21 | 60.6 | 1 | 0 |
| 2577 | FF | 494095.73 | 6926112.21 | 60.6 | 1 | 0 |
| 2578 | GF | 494104.12 | 6926117.77 | 59.7 | 0 | 0 |
| 2578 | FF | 494104.12 | 6926117.77 | 59.7 | 1 | 0 |
| 2579 | GF | 494112.89 | 6926124.38 | 58.6 | 0 | 0 |
| 2579 | FF | 494112.89 | 6926124.38 | 58.6 | 1 | 0 |
| 2580 | GF | 494133.49 | 6926169.19 | 54.9 | 0 | 0 |
| 2580 | FF | 494133.49 | 6926169.19 | 54.9 | 0 | 0 |
| 2581 | GF | 494057.23 | 6926173.95 | 58.5 | 0 | 0 |
| 2581 | FF | 494057.23 | 6926173.95 | 58.5 | 1 | 0 |
| 2582 | GF | 494072.63 | 6926199.74 | 55.6 | 0 | 0 |
| 2582 | FF | 494072.63 | 6926199.74 | 55.6 | 0 | 0 |
| 2583 | GF | 494090.3 | 6926195.88 | 55.5 | 0 | 0 |
| 2583 | FF | 494090.3 | 6926195.88 | 55.5 | 0 | 0 |
| 2584 | GF | 494602.95 | 6925840.23 | 52.4 | 0 | 0 |
| 2584 | FF | 494602.95 | 6925840.23 | 52.4 | 0 | 0 |
| 2585 | GF | 494588.75 | 6925795.65 | 55.1 | 0 | 0 |
| 2585 | FF | 494588.75 | 6925795.65 | 55.1 | 0 | 0 |
| 2586 | GF | 494862.69 | 6925716.35 | 51.9 | 2 | 2 |
| 2586 | FF | 494862.69 | 6925716.35 | 51.9 | 2 | 2 |
| 2587 | GF | 494885.54 | 6925874.58 | 49.5 | 0 | 0 |
| 2587 | FF | 494885.54 | 6925874.58 | 49.5 | 0 | 0 |
| 2588 | GF | 494885.83 | 6925861.31 | 49.6 | 0 | 0 |
| 2588 | FF | 494885.83 | 6925861.31 | 49.6 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 2589 | GF | 494928.16 | 6925961.13 | 49.8 | 0 | 0 |
| 2589 | FF | 494928.16 | 6925961.13 | 49.8 | 0 | 0 |
| 2590 | GF | 494957.35 | 6925963.5 | 51.7 | 0 | 0 |
| 2590 | FF | 494957.35 | 6925963.5 | 51.7 | 0 | 0 |
| 2591 | GF | 494933.25 | 6925815.55 | 50.1 | 1 | 1 |
| 2591 | FF | 494933.25 | 6925815.55 | 50.1 | 1 | 1 |
| 2592 | GF | 494933.92 | 6925787.86 | 50.2 | 2 | 2 |
| 2592 | FF | 494933.92 | 6925787.86 | 50.2 | 2 | 2 |
| 2593 | GF | 494964.11 | 6925832.05 | 51.2 | 1 | 1 |
| 2593 | FF | 494964.11 | 6925832.05 | 51.2 | 1 | 1 |
| 2594 | GF | 495005.34 | 6925861.09 | 51.4 | 1 | 1 |
| 2594 | FF | 495005.34 | 6925861.09 | 51.4 | 1 | 1 |
| 2595 | GF | 495017.16 | 6925860.67 | 51.3 | 1 | 1 |
| 2595 | FF | 495017.16 | 6925860.67 | 51.3 | 1 | 1 |
| 2596 | GF | 495003.14 | 6925887.93 | 52.9 | 1 | 0 |
| 2596 | FF | 495003.14 | 6925887.93 | 52.9 | 1 | 1 |
| 2597 | GF | 495017 | 6925887.25 | 53.2 | 1 | 0 |
| 2597 | FF | 495017 | 6925887.25 | 53.2 | 1 | 1 |
| 2598 | GF | 495005.95 | 6925929.35 | 52.3 | 0 | 0 |
| 2598 | FF | 495005.95 | 6925929.35 | 52.3 | 0 | 0 |
| 2599 | GF | 495073.58 | 6925994.86 | 52.0 | 0 | 0 |
| 2599 | FF | 495073.58 | 6925994.86 | 52.0 | 1 | 0 |
| 2600 | GF | 495119.09 | 6925940.41 | 52.0 | 1 | 0 |
| 2600 | FF | 495119.09 | 6925940.41 | 52.0 | 1 | 1 |
| 2601 | GF | 495151.54 | 6925930.63 | 50.8 | 1 | 0 |
| 2601 | FF | 495151.54 | 6925930.63 | 50.8 | 1 | 1 |
| 2602 | GF | 495139.86 | 6925916.13 | 51.0 | 1 | 0 |
| 2602 | FF | 495139.86 | 6925916.13 | 51.0 | 1 | 1 |
| 2603 | GF | 493918.28 | 6926241.5 | 60.5 | 2 | 2 |
| 2603 | FF | 493918.28 | 6926241.5 | 60.5 | 2 | 2 |
| 2603 | GF | 495168.77 | 6925875.48 | 49.7 | 3 | 1 |
| 2603 | FF | 495168.77 | 6925875.48 | 49.7 | 3 | 3 |
| 2604 | GF | 494880.2 | 6925962.36 | 49.6 | 0 | 0 |
| 2604 | FF | 494880.2 | 6925962.36 | 49.6 | 0 | 0 |



| Lot | Floor | Coordinates and ground elevation at centre of lot, m | | | QDC MP4.4 Road Traffic Noise Category (Worst-case on lot) | |
|------|-------|--|------------|---------------------|---|---------------|
| | | Easting | Northing | Ground elevation, m | No Mitigation | Noise Barrier |
| 2604 | GF | 495178.54 | 6925885.59 | 49.6 | 3 | 1 |
| 2604 | FF | 495178.54 | 6925885.59 | 49.6 | 3 | 3 |
| 2605 | GF | 494879.36 | 6925973.42 | 49.7 | 0 | 0 |
| 2605 | FF | 494879.36 | 6925973.42 | 49.7 | 0 | 0 |





Appendix D Reference Noise Barrier Designs

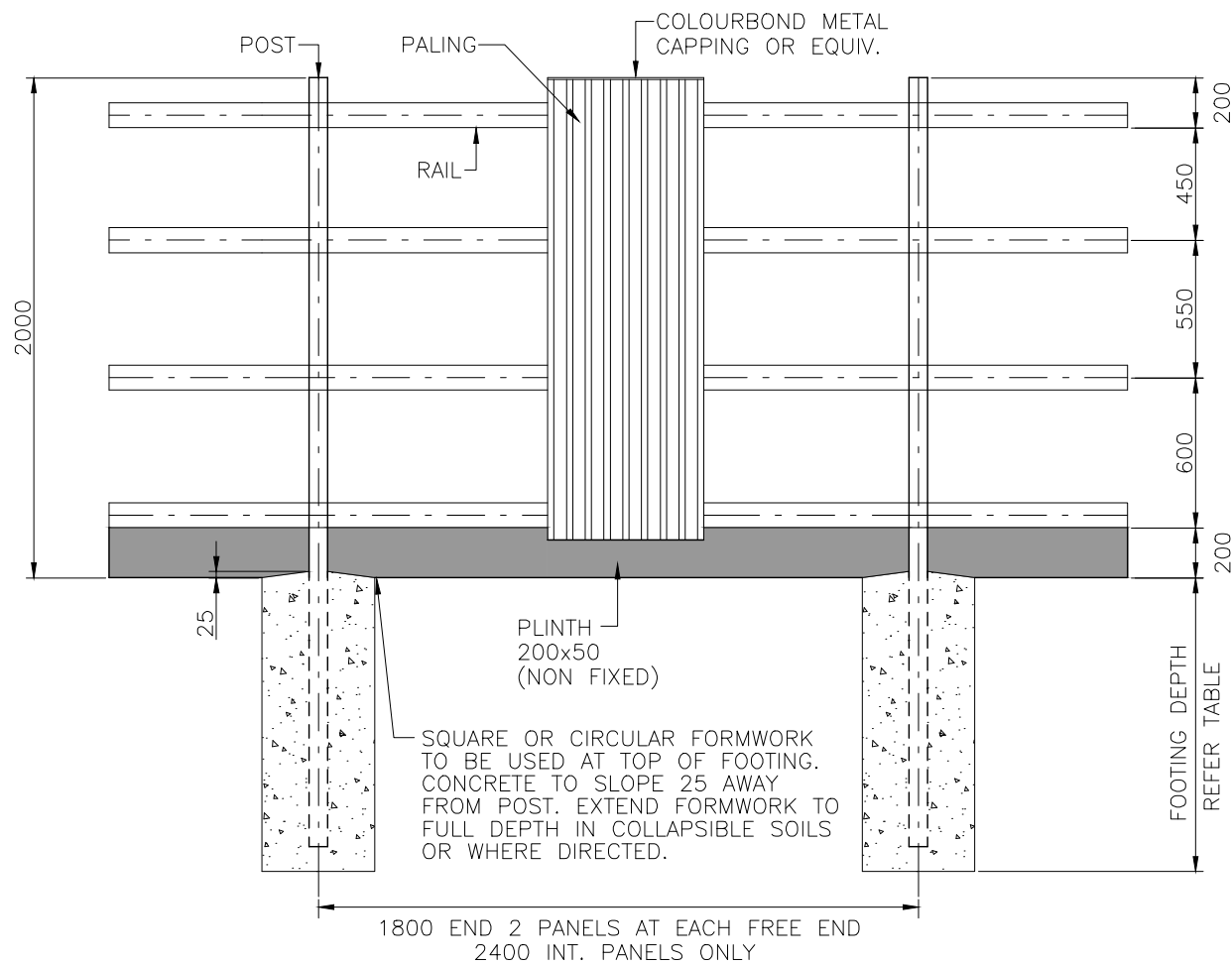
Flagstone Development, Stage 4

Road Traffic Noise Intrusion Assessment

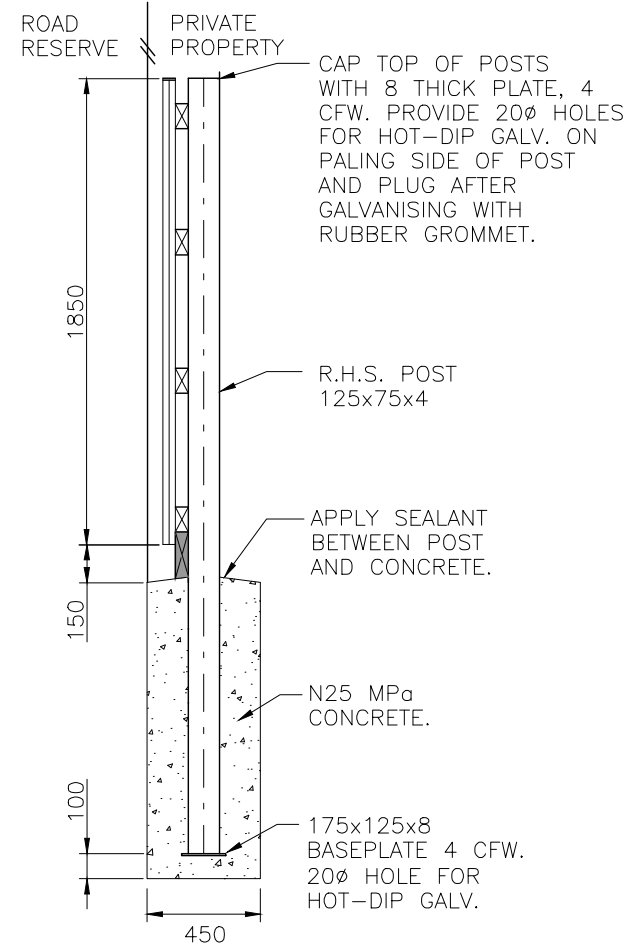
Peet Flagstone City Pty Ltd

SLR Project No.: 620.v10512.02006

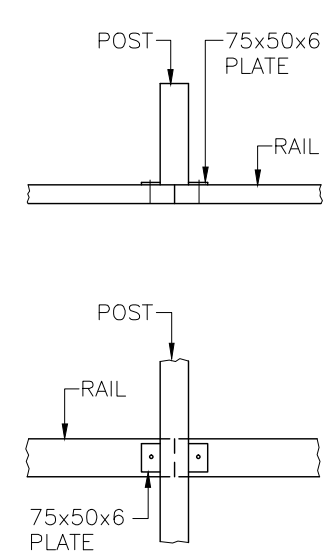
25 October 2024



ELEVATION



SECTION

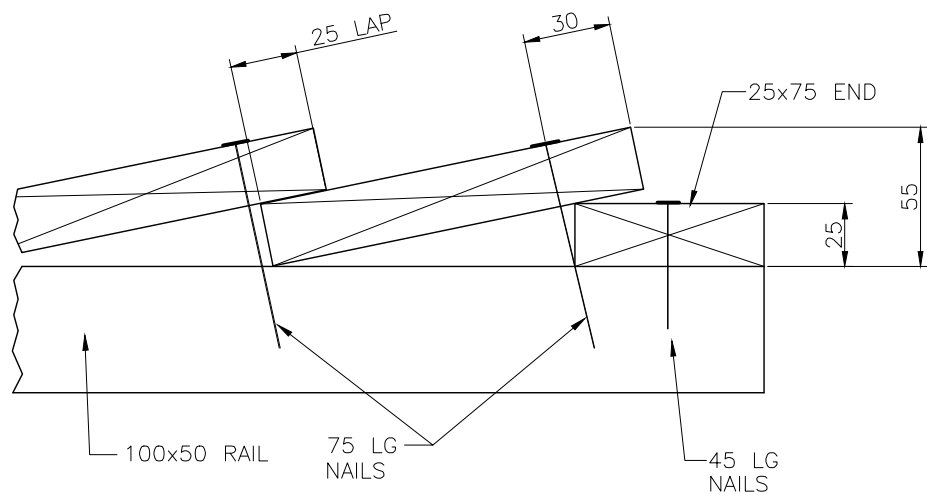


POST & RAIL CONNECTION

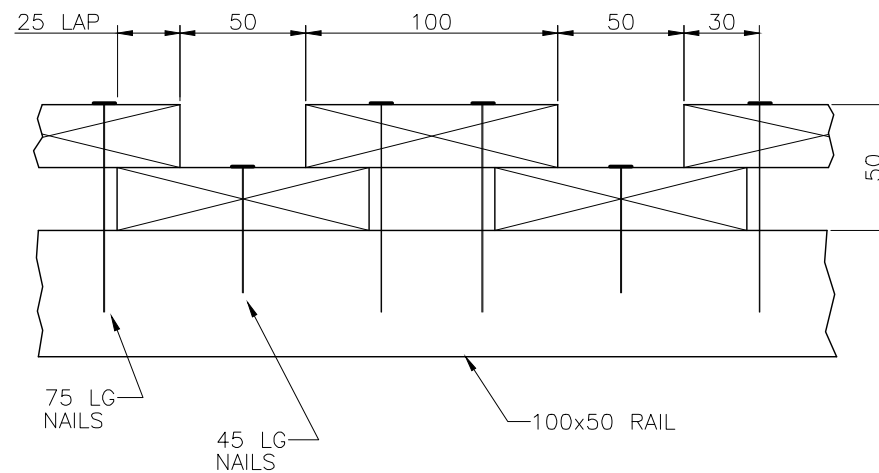
| FOOTING DEPTH | |
|--------------------------------|---------------|
| SOIL TYPE | FOOTING DEPTH |
| SOFT CLAY (Cu = 25 kPa) | 1600 |
| FIRM CLAY (Cu = 50 kPa) | 1300 |
| STIFF CLAY (Cu = 100 kPa) | 1100 |
| MEDIUM DENSE NON-COHESIVE SOIL | 1200 |

NOTES:

- THIS DRAWING DEPICTS A TYPICAL 2000 HIGH ACOUSTIC BARRIER AND DOES NOT NECESSARILY REPRESENT A NOISE ATTENUATION SOLUTION FOR ALL DEVELOPMENTS. NOISE ATTENUATION SOLUTION FOR EACH DEVELOPMENT IS SITE SPECIFIC AND SHALL BE ADDRESSED IN ACCORDANCE WITH THE "NOISE IMPACT ASSESSMENT PLANNING SCHEME POLICY" OF THE BRISBANE CITY PLAN.
- MAXIMUM PERMISSIBLE STRESS DESIGN WIND VELOCITY IS 33m/s (W33) WHICH CORRESPONDS TO A SUBURBAN ENVIRONMENT WITH NO EXPOSURE TO OPEN AREAS AND NOT LOCATED IN CLOSE PROXIMITY TO HILLS, RIDGES OR ESCARPMENTS, AS THE NATURAL SURFACE 2m EITHER SIDE OF THE FENCE IS ASSUMED FLAT FOR DESIGN OF FOOTING. IF THESE CONDITIONS ARE NOT MET AN ALTERNATIVE CERTIFIED ENGINEERING DESIGN MUST BE SUBMITTED FOR APPROVAL.
- FOR NEW SUBDIVISIONS/DEVELOPMENTS, THE ENTIRE FENCE SHALL BE CONTAINED WITHIN THE PRIVATE PROPERTY AND MAINTAINED BY THE PROPERTY OWNER.
- ALL PALINGS, RAILS AND PLINTH SHALL BE C.C.A TREATED PINE TO H5 LEVEL IN ACCORDANCE WITH AS 1604.
- ALL FIXINGS SHALL BE HOT-DIP GALVANISED OR EQUIVALENT.
- CAPPING: COLOURBOND METAL, 30 DEEP WITH OVERFOLDED EDGES FIX WITH No.10 x 12 LONG GALV. TYPE 17 SCREWS AT 300 CRS AND STAGGERED EACH SIDE. CAPPING TO FIT SNUGLY OVER PALINGS.
- PALINGS: F5 TREATED PINE. REFER PALING DETAILS FOR SIZES. NAILS SHALL BE 2.8Ø HOT-DIP GALVANISED FLAT HEAD CLOUTS (OR SIMILAR GUN-DRIVEN NAILS). STAGGER NAIL PATTERN ALONG LENGTH OF PALING TO AVOID SPLITTING AND DRIVE NAILS SQUARE TO FACE OF BOARD. RING SHANK NAILS TO BE USED.
- RAILS: 100 x 50 F5 TREATED PINE. FIX WITH No.14-10 x 50 GALVANISED HEX HD TYPE 17 SCREW.
- POSTS: 125 x 75 x 4 R.H.S. HOT-DIP GALVANISED AFTER FABRICATION.
- PLINTH: 200 x 50 F5 TREATED PINE (NON FIXED).
- DIMENSIONS IN MILLIMETRES (UNO).



SYSTEM 1 (150x25)



SYSTEM 2 (100x25)

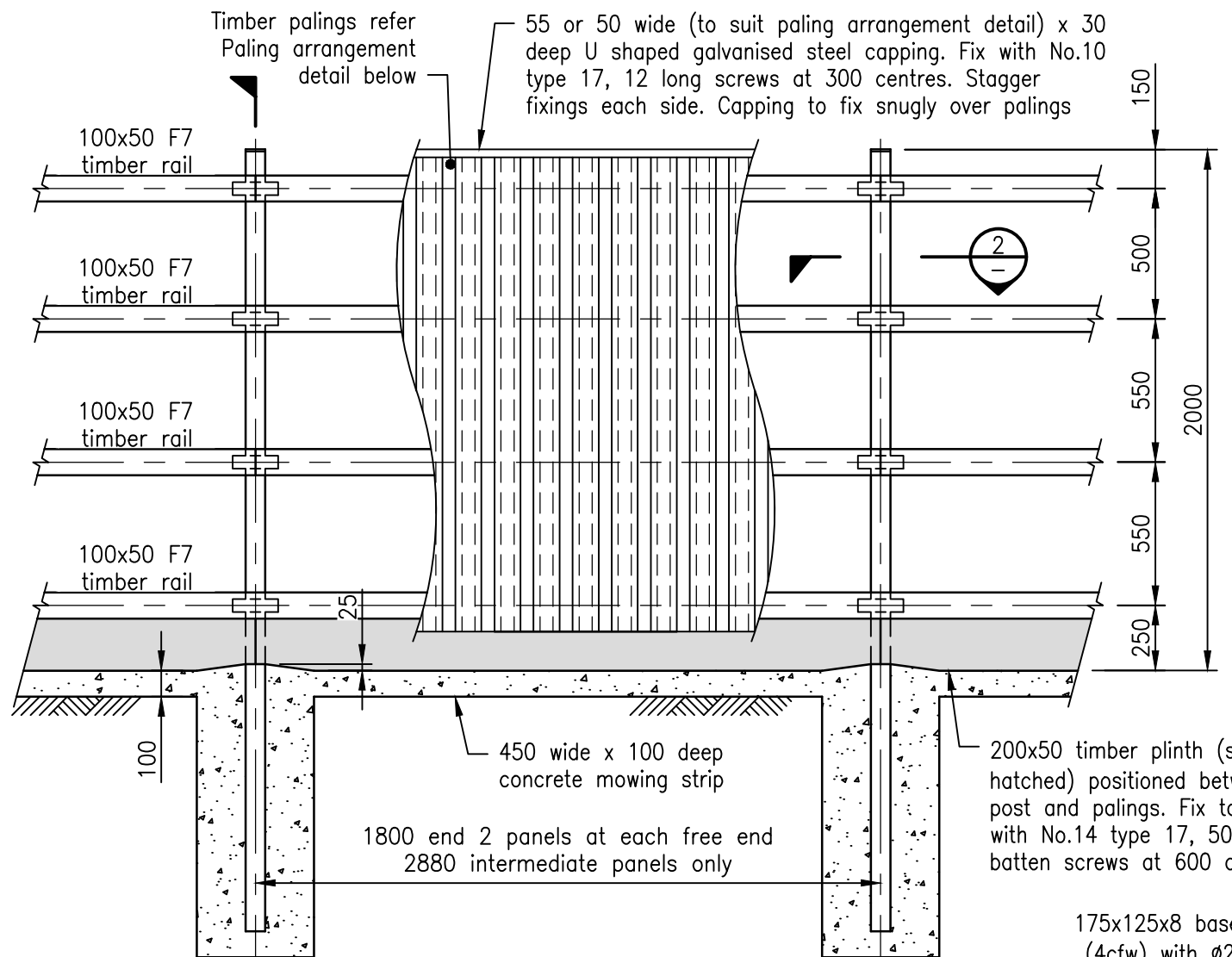
TREATED PINE PALING

| ISSUE | AMENDMENT | DRAWN DATE | CHK'D DATE | APPR'D DATE |
|-------|--|------------|------------|-------------|
| B | Drawing Title Amended | FEB '16 | JUL '16 | JUL '16 |
| A | Drawing Converted from UMS Series April 2014 | APR '14 | APR '14 | APR '14 |

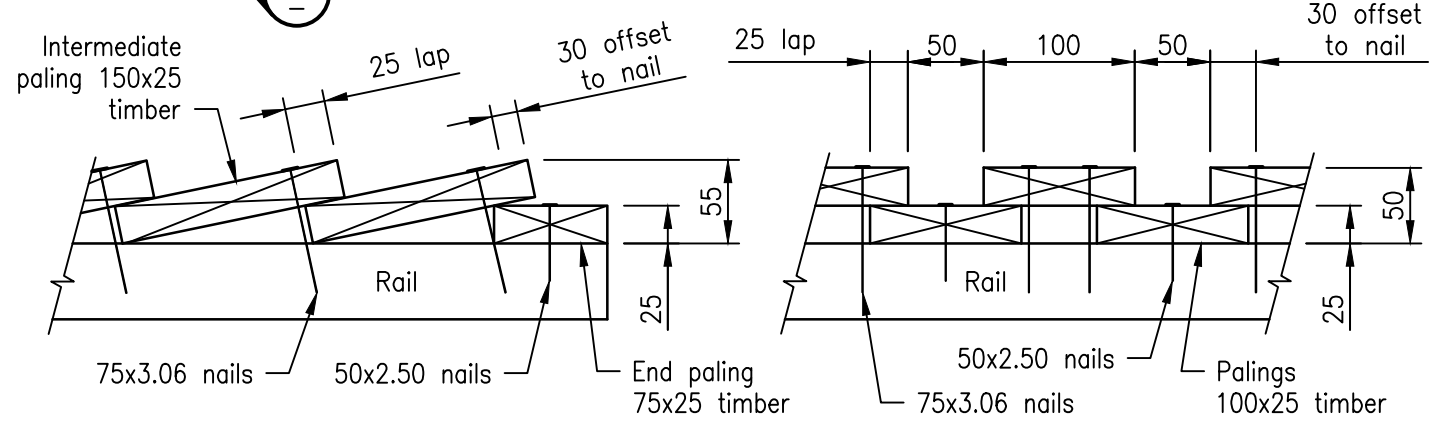
| | | | |
|--|--|------|-----------|
| DRAWING AUTHORISED FOR PUBLICATION B.BALL SIGNATURE ON ORIGINAL DATED 29/06/01 R.P.E.Q 3852 | | | |
| DESIGN | Std Dwgs Group | DATE | APRIL '01 |
| DRAWN | CITY DESIGN | DATE | APRIL '01 |
| CHECKED | M. STEER | DATE | MAY '01 |
| DRAWING FILENAME | BSD-7021 (B) Noise barrier fence 2.0m high - Post and paling.dwg | | |
| ASSOCIATED PLANS | SUPERSEDES UMS-245 | | |
| ASSET ENGINEERING MANAGER STRATEGIC ASSET MANAGEMENT DESIGN APPROVED B.HANSEN SIGNATURE ON ORIGINAL DATED 27/06/01 | | | |
| PRINCIPAL ASSET OFFICER ROADS & DRAINAGE | | | |



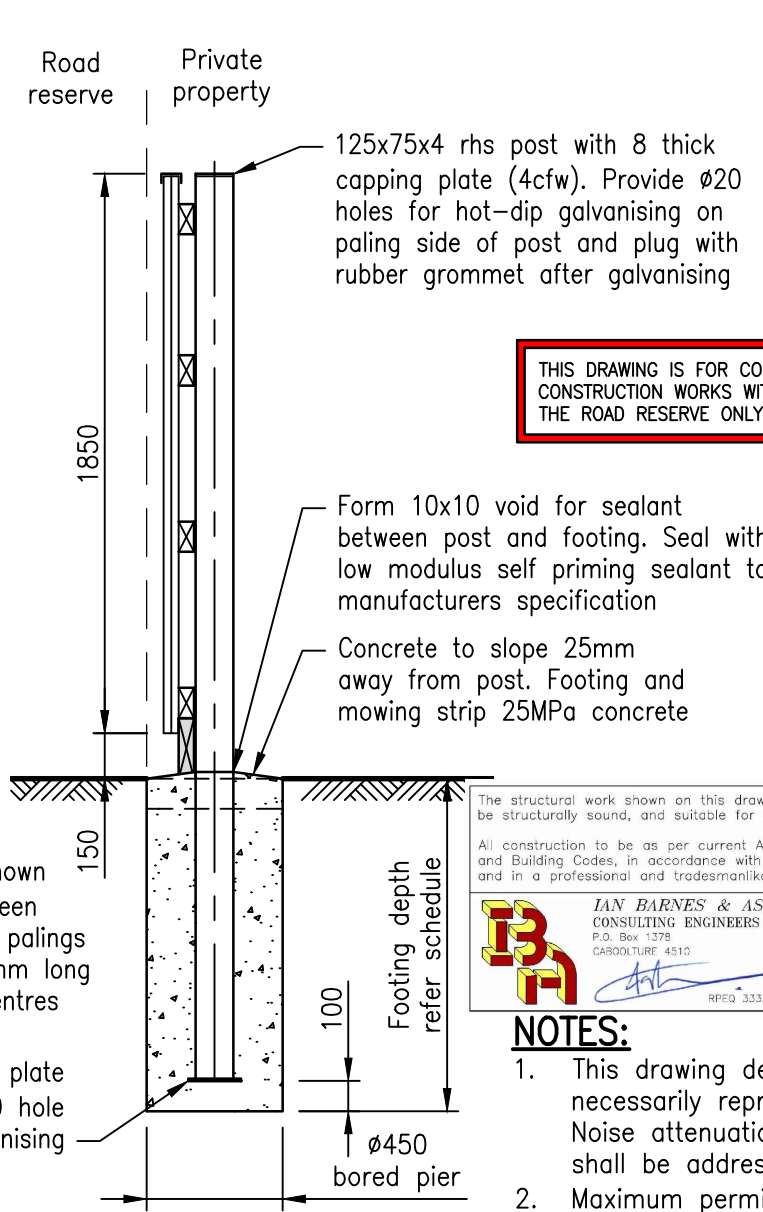
| BRISBANE CITY COUNCIL STANDARD DRAWING | |
|---|--------------|
| NOISE BARRIER FENCE 2.0m HIGH POST AND PALING | |
| SCALE | NOT TO SCALE |
| DWG No. | BSD-7021 |
| ORIGINAL SIZE | A3 |
| REVISION | B |



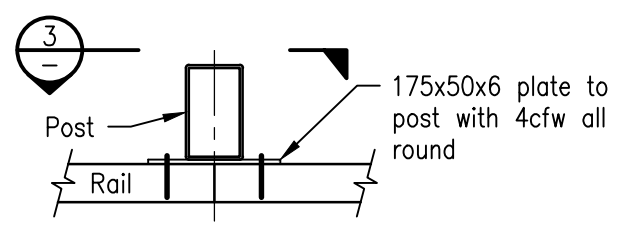
ELEVATION
Scale A



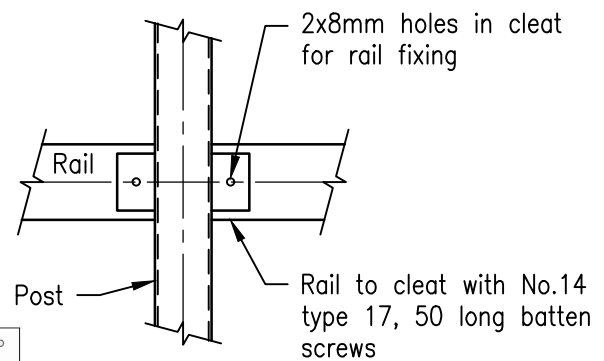
PALING ARRANGEMENT DETAILS
Scale C



SECTION 1
Scale A



SECTION 3
Scale B



SECTION 2
Scale B

THIS DRAWING IS FOR COUNCIL CONSTRUCTION WORKS WITHIN THE ROAD RESERVE ONLY

The structural work shown on this drawing is considered to be structurally sound, and suitable for the design loads.

All construction to be as per current Australian Standards and Building Codes, in accordance with MBRC requirements, and in a professional and tradesmanlike manner

IAN BARNES & ASSOCIATES P/Ltd
CONSULTING ENGINEERS
P.O. Box 1378
CAROLURUP 4510

ABN 70 057802490
Office : 07 5485 8444
Mobile : 0418 873 320

RPEQ 3333 Date : 13/09/2017

NOTES:

- This drawing depicts a typical 2000 high acoustic barrier and does not necessarily represent a noise attenuation solution for all developments. Noise attenuation solution for each development is site specific and shall be addressed by a qualified acoustic engineer.
- Maximum permissible stress design wind velocity is 33m/s (w33) which corresponds to a suburban environment with no exposure to open areas and not located in close proximity to hills, ridges or escarpments, as the natural surface 2m either side of the fence is assumed flat for design of footing. If these conditions are not met an alternative certified engineering design must be submitted for approval.
- For new subdivisions/developments, the entire fence shall be contained within the private property and maintained by the property owner.
- All palings, rails and plinths shall be ACQ or CCA treated pine to H5 level in accordance with AS 1604. Rails min. F7 Stress Grade.
- All fixings (apart from nails) shall be 'Zenith-Tufcote' or 'Buildex-Climacoat' or approved equivalent (unless noted otherwise).
- All nails shall be ring shank type and hot dipped galvanised.
- Stagger nail pattern along length of paling to avoid splitting and drive nails square to face of board.
- Posts shall be hot-dip galvanised after fabrication.
- Noise barrier fence shall be screened with vegetation.
- Dimensions are in millimetres unless stated otherwise.

FOOTING DEPTH SCHEDULE

| SOIL TYPE | FOOTING DEPTH |
|---------------------------------------|---------------|
| Soft clay (Cu = 25kPa) | 1600 |
| Firm clay (Cu = 50kPa) | 1300 |
| Stiff clay (Cu = 100kPa) | 1100 |
| Medium dense non-cohesive soil medium | 1200 |

| REVISIONS | INIT | DATE |
|-----------|---|----------|
| E | | |
| D | | |
| C | Approved by Structural Engineer | TC 7/17 |
| B | Structural Design Note Changed | RH 12/16 |
| A | Add note - For council construction works only, change landscape note | BW 08/16 |
| X | ORIGINAL ISSUE | BW 07/16 |

| SCALES |
|--------------------------------|
| A 0mm 100 200 300 400 500 1:25 |
| B 0mm 50 100 150 200 1:10 |
| C 0mm 25 50 75 100 1:5 |

| Drawn | BW | Date | 07/16 |
|--|----|------|-------|
| Coordinator | PP | Date | 07/16 |
| AUTHORISED | | | |
| SYD JERRAM | | | |
| 07/07/16 | | | |
| Manager Integrated Transport Planning & Design | | | |
| RPEQ 6872 | | | |

**NOISE BARRIER FENCE
2.0m HIGH POST AND PALING**

Moreton Bay Regional Council

DRG No. **SF-1520**

ORIGINAL SIZE **A3** REVISION **C**



Appendix E Glossary of Terms

Flagstone Development, Stage 4

Road Traffic Noise Intrusion Assessment

Peet Flagstone City Pty Ltd

SLR Project No.: 620.v10512.02006

25 October 2024

Sound Level (or Noise Level)

The terms sound and noise are almost interchangeable, except that in common usage noise is often used to refer to unwanted sound.

Sound (or noise) consists of minute fluctuations in atmospheric pressure capable of evoking the sense of hearing. The human ear (and those of other species) responds to changes in sound pressure over a very wide range. The loudest sound pressure to which the human ear responds is ten million times greater than the softest. The decibel (dB or dBL) scale reduces this ratio to a more manageable size by the use of logarithms.

A-weighted Sound Pressure Level

The overall level of a sound is usually expressed in terms of dBA, which is measured using a sound level meter with an 'A-weighting' filter. This is an electronic filter having a frequency response corresponding approximately to human hearing.

Change in Sound Pressure Levels

For human perception, a change of 1 dBA or 2 dBA in the level of a sound is considered to be indiscernible, while a 3 dBA to 5 dBA change corresponds to a small but noticeable change in loudness. A 10 dBA change corresponds to an approximate doubling or halving in loudness. As noted in Section 2.4 of the TMR CoP Vol 1, while the above noted changes in sound pressure level are *not precisely verifiable for road traffic noise, it is useful in understanding the significance of change in environmental noise exposure.*

Additional facts about road traffic noise as stated in Section 2.4 of the TMR CoP Vol 1:

- A 3 dBA change in noise level is equivalent to halving or doubling the traffic volumes.
- A 10 dBA change in noise level is equivalent to halving or doubling the subjective or perceived loudness or a tenfold increase or decrease in traffic volume.
- A 10 km/h increase in speed will increase the noise level by approximately 1 dBA.
- A 3.5% compound annual growth rate in traffic will increase the noise level by approximately 1.5 dBA over a 10-year horizon.
- An 8% compound annual growth rate in traffic will increase the noise level by approximately 3.0 dBA over a 10-year horizon.

Typical Sound Pressure Levels

The table below lists examples of typical sound pressure levels.

Table D-1 Examples of Perceived Sound Pressure Levels

| Sound pressure level (dBA) | Typical Example |
|----------------------------|---|
| 130 | Threshold of pain |
| 120 | Metal hammering |
| 110 | Grinding on steel |
| 100 | Loud car horn at 3 metres (m) |
| 90 | Dog bark at 1 m |
| 80 | Cicadas at 1 m |
| 70 | Noise level directly adjacent to a busy main road |
| 60 | Ambient noise level in urban area close to main roads |



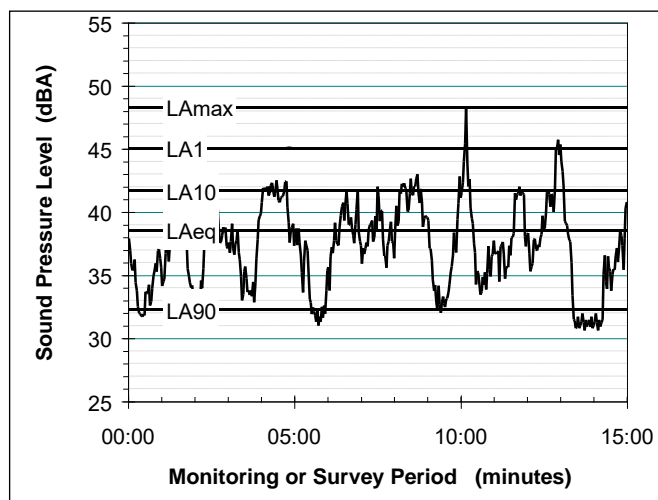
| Sound pressure level (dBA) | Typical Example |
|----------------------------|---|
| 50 | Day time in a quiet suburban environment with background or distant road traffic noise |
| 40 | Night-time in a quiet suburban environment with background or distant road traffic noise Ambient noise level in rural to semi-rural environments with light breezes and some noise from insects, birds and distant traffic |
| 30 | Ambient noise level in a typical rural noise environment in the absence of insect noise and wind. Inside bedroom |
| 20 | Ambient noise level in remote rural environment away from main roads with no wind and no insect noise |

Statistical Noise Levels

Sounds that vary in level over time, such as road traffic noise and most community noise, are commonly described in terms of the statistical exceedance levels (LAN), where LAN is the A-weighted sound pressure level exceeded for N% of a given measurement period. For example, the LA1 is the noise level exceeded for 1% of the time and LA10 the noise exceeded for 10% of the time.

Figure D-1 below presents a hypothetical 15-minute noise measurement, illustrating various common statistical indices of interest.

Figure D-1 Hypothetical 15-minute Noise Measurement



Of particular relevance to this study, are:

- LA10: The A-weighted noise level exceeded for 10% during any given measurement period. This is commonly referred to as the average maximum noise level.

Additionally;

- LA10(18hour) Road Traffic Noise Level: the level exceeded for 10% of any measurement period; the usual period of measurement is 1 hour. The hourly LA10 level, therefore, is the traffic noise level exceeded for 6 minutes in the hour. The 18-hour LA10 level



(LA_{10(18hour)}) is the arithmetic average of 18, hourly LA₁₀ traffic noise levels measured in consecutive hours between 6:00 am and 12:00 midnight.

- LA_{10(12hour)} Road Traffic Noise Level – is the arithmetic average of 12 hourly LA₁₀ traffic noise levels measured in consecutive hours between 6:00 am and 6:00 pm.
- LA_{1(1hour)} Road Traffic Noise Level – the level exceeded for n% of a 1-hour period.

Noise Propagation

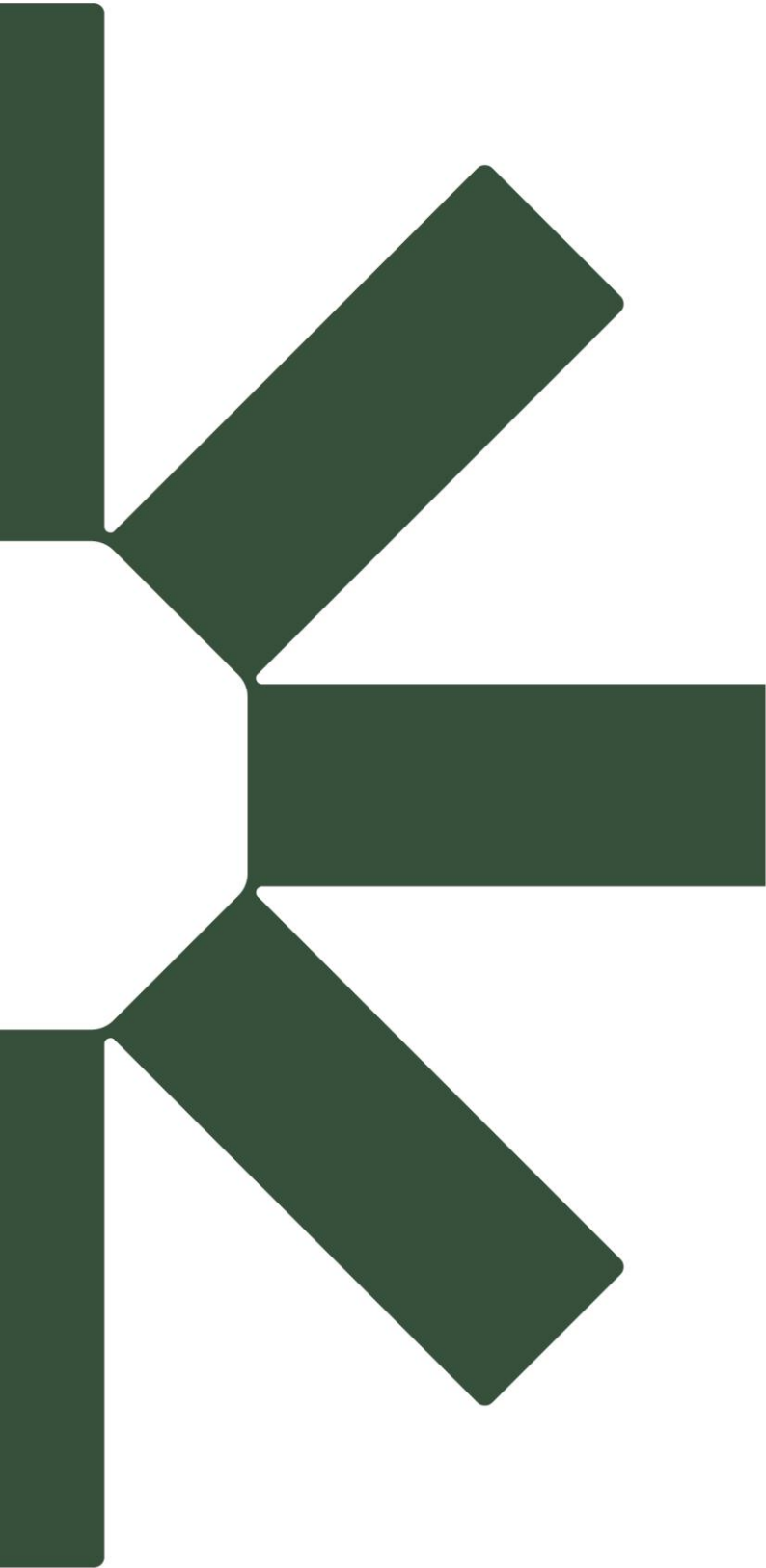
Provided the receptor is in the far-field of the noise source, noise levels will reduce as a receptor moves further away from the source. This is due to spreading of the noise source energy over distance. For a simple point source (for example, a motor) the theoretical reduction in noise levels is 6 dBA per doubling of distance. For a line source (for example, a busy road) the theoretical reduction is 3 dBA per doubling of distance. In reality however other factors affect noise propagation. These include ground absorption, air absorption, acoustic screening, and meteorological effects.

Facade Corrected versus Free field

A 'facade corrected' measurement/monitoring location is a location which is influenced by facade reflections. Measurements at facades are typically taken at a distance of 1 m away and the measured noise level generally regarded as being +2.5 dB higher than in the 'free field'.

A 'free field' measurement/monitoring location is a location where the microphone is positioned sufficiently far from nearby surfaces for the measured data to not be influenced by reflected noise. This is typically regarded as a position 3.5 m or greater from a reflective surface.





Making Sustainability Happen